

SECTION 22, TOWNSHIP 31 N., RANGE 5 E., W.M.

PROJECT INFO:

WATER SERVICE: CITY OF ARLINGTON
SEWER SERVICE: CITY OF ARLINGTON
FIRE DISTRICT: CITY OF ARLINGTON
PROJECT AREA: 2.39 ACRES / 104,193 S.F.
SITE ADDRESS: ARLINGTON AIRPORT LOTS 012 & 013
ZONING/COMP. PLAN: AF-AVIATION FLIGHTLINE
PROPOSED LAND USE: 10,400 STORAGE OF AIRCRAFT
DATUM: NAVD 88 / NAD 83
PARKING REQ: 1 SPACE / 2 HANGERS (INSIDE HANGERS)
LANDSCAPE: 45,231 S.F.
BUILDING HEIGHT: BUILDING A 18.5' / BUILDING B 20.5'
BUILDING SETBACKS: 5 FEET

LIGHTING NOTE:

THE CONTRACTOR SHALL INSTALL LOW SODIUM OR SIMILAR LOW INTENSITY ILLUMINATION LIGHTING AND SHALL BE DOWN SHIELDED.

DEVELOPER:

CARRERA HANGARS LP
KIM BERGEMEIER
17912 59TH DRIVE N.E.
ARLINGTON, WA 98223
(425) 418-3923

CONTACT / ENGINEER:

HALE MILLIGAN & ASSOCIATES LLC
KELLI HALE, PE
307 N OLYMPIC AVE, SUITE 209
ARLINGTON, WA 98223
(360) 474-4624

LEGAL DESCRIPTION

LEASE EASEMENT LOT 012

COMMENCING AT THE NORTH QUARTER CORNER OF SAID SECTION 22; THENCE S 4°30'47" E ALONG THE NORTH-SOUTH CENTERLINE OF SAID SECTION A DISTANCE OF 3,562.17 FEET; THENCE S 85°36'29" W A DISTANCE OF 240.41 FEET TO THE TRUE POINT OF BEGINNING; THENCE CONTINUE S 85°36'29" W A DISTANCE OF 455.24 FEET; THENCE S 4°23'31" E A DISTANCE OF 116.00 FEET; THENCE N 85°36'29" E A DISTANCE OF 455.00 FEET; THENCE N 4°16'25" E A DISTANCE OF 116.00 FEET TO THE TRUE POINT OF BEGINNING.

SITUATE IN THE CITY OF ARLINGTON, COUNTY OF SNOHOMISH, STATE OF WASHINGTON.

CONTAINING 52,794 SQUARE FEET.

LEASE EASEMENT LOT 013

COMMENCING AT THE NORTH QUARTER CORNER OF SAID SECTION 22; THENCE S 4°30'47" E ALONG THE NORTH-SOUTH CENTERLINE OF SAID SECTION A DISTANCE OF 3,562.17 FEET; THENCE S 85°36'29" W A DISTANCE OF 612.01 FEET; THENCE N 4°28'30" W A DISTANCE OF 28.00 FEET TO THE TRUE POINT OF BEGINNING; THENCE N 4°28'30" W A DISTANCE OF 485.00 FEET; THENCE S 85°31'30" W A DISTANCE OF 105.98 FEET; THENCE S 4°28'30" E A DISTANCE OF 485.00 FEET.; THENCE N 85°31'30" E A DISTANCE OF 105.98 FEET TO THE TRUE POINT OF BEGINNING.

SITUATE IN THE CITY OF ARLINGTON, COUNTY OF SNOHOMISH, STATE OF WASHINGTON.

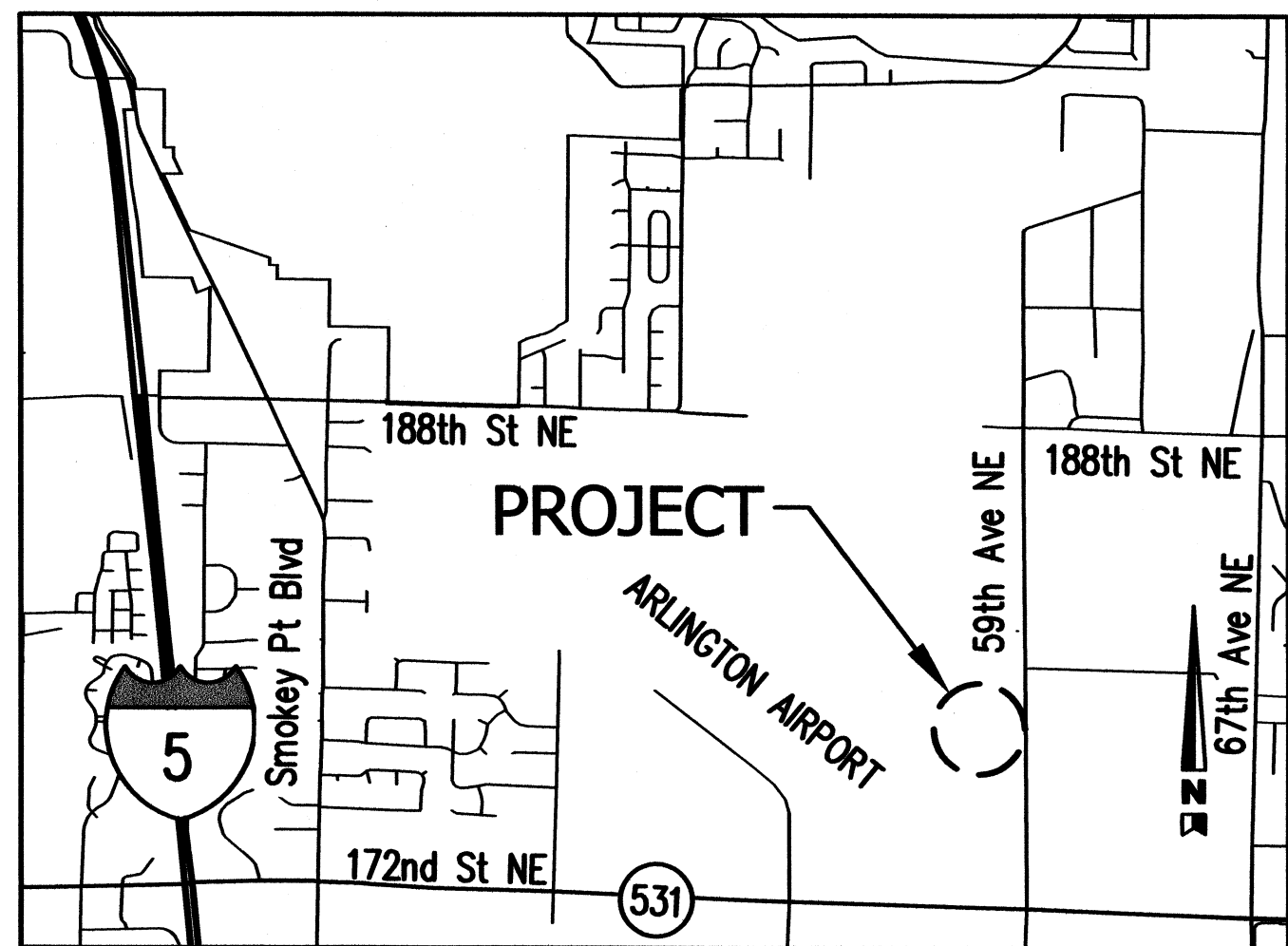
CONTAINING 51,399 SQUARE FEET.

SURVEY NOTES:

BOUNDARY LINE SURVEY COMPLETED FOR CITY OF ARLINGTON AIRPORT BY CASCADE SURVEYING AND ENGINEERING. PROPERTY CORNERS HAVE BEEN STAKED BY CASCADE SURVEYING AND ENGINEERING.

TOPOGRAPHIC SURVEY COMPLETED BY HUEY SURVEYING AND LAND CONSULTING, INC.

EXISTING HANGERS AND TAXILANES DIGITIZED FROM AERIAL PHOTOGRAPH. CONTOURS ARE FROM LIDAR.



VICINITY MAP
N.T.S.

SHEET INDEX

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ESTIMATED GRADING QUANTITIES:

CUT: 83 CU. YDS.
FILL: 197 CU. YDS.

IMPERVIOUS AREA:

	EXISTING:	PROPOSED:	CONSTRUCTED:
ENTRANCES	25,142 S.F.	24,717 S.F.	27,723 S.F.
BUILDINGS	28,393 S.F.	43,469 S.F.	44,310 S.F.
TOTAL	51,535 S.F.	68,186 S.F.	72,033 S.F.

PERVIOUS AREA:

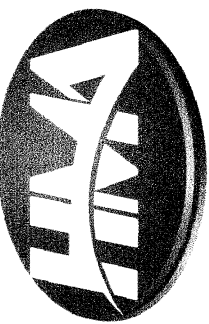
GRASS	28,419 S.F.	32,266 S.F.
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AS-BUILT DRAWING APPROVAL
ACKNOWLEDGMENT

THIS AS-BUILT PLAN SHEET HAS BEEN REVIEWED AND ACCEPTED AS A RECORD DRAWING. THE LICENSED DESIGN ENGINEER WHOSE STAMP AND SIGNATURE APPEAR ON THIS SHEET ASSUMES FULL AND COMPLETE RESPONSIBILITY FOR THE ACCURACY THEREIN.

BY: [Signature]
City Engineer, City of Arlington
DATE: 01-29-14

HALE MILLIGAN & ASSOCIATES
ENGINEERING
307 N Olympic Avenue Suite 209 Arlington, WA 98223
P 360.474.4624 F 425.968.745



DATE	NO.	REVISION	BY
1/28/14	5	AS-BUILT CITY COMMENTS	KSH
12/23/13	4	AS-BUILT	KSH
01/05/12	3	CITY COMMENTS	KSH
12/28/11	2	CITY COMMENTS	KSH
12/12/11	1	CITY COMMENTS	KSH
DESIGNED BY:			KSH
DRAWN BY:			KSH/ASM

CARRERA PACIFIC HANGARS
ARLINGTON AIRPORT LOT 12 & 13
COVER

20110017

1 OF 7

GENERAL CONSTRUCTION NOTES

1. ALL WORK AND MATERIALS SHALL CONFORM TO THE CURRENT EDITION OF THE CITY OF ARLINGTON PUBLIC WORKS STANDARDS AND SPECIFICATIONS, AND THE CURRENT EDITION OF THE WASHINGTON STATE DEPARTMENT OF TRANSPORTATION (WSDOT) STANDARD SPECIFICATIONS FOR ROAD, BRIDGE, AND MUNICIPAL CONSTRUCTION. A COPY OF THESE DOCUMENTS SHALL BE ON SITE DURING CONSTRUCTION.
2. IT IS THE SOLE RESPONSIBILITY OF THE DEVELOPER/CONTRACTOR TO OBTAIN A GRADING PERMIT, RIGHT-OF-WAY PERMIT, AND UTILITY PERMITS, FROM THE CITY. ALL REQUIRED PERMITS FROM OTHER AGENCIES MUST ALSO BE OBTAINED BY THE DEVELOPER/CONTRACTOR.
3. PRIOR TO ANY CONSTRUCTION ACTIVITY, THE DEVELOPER/ CONTRACTOR SHALL ATTEND A PRECONSTRUCTION CONFERENCE WITH THE CITY. THE CONTRACTOR SHALL SCHEDULE THE PRE-CONSTRUCTION CONFERENCE BY CALLING (509) 403-3500. PRIOR TO SCHEDULING, THE CONTRACTOR MUST SUBMIT AND RECEIVE APPROVAL FOR THE TRAFFIC CONTROL PLAN, CITY PERMITS, TEMPORARY EROSION AND SEDIMENT CONTROL PLAN, PERFORMANCE BOND, COPY OF OTHER AGENCY PERMITS, A COPY OF THE CONTRACTOR'S LICENSE, AND PROOF OF INSURANCE COVERAGE.
4. A COPY OF THE APPROVED CONSTRUCTION PLANS MUST BE ON THE JOB SITE WHEN CONSTRUCTION IS IN PROGRESS.
5. ALL SITE WORK SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE APPROVED PLANS. ANY DEVIATION FROM THE APPROVED PLANS WILL REQUIRE PRIOR APPROVAL FROM THE OWNER, THE CITY ENGINEER, AND OTHER APPROPRIATE PUBLIC AGENCIES.
6. ALL OF THE LOCATIONS OF THE EXISTING UTILITIES SHOWN IN THE PLANS HAVE BEEN ESTABLISHED BY FIELD SURVEY OR OBTAINED FROM AVAILABLE RECORDS AND SHALL THEREFORE BE CONSIDERED APPROXIMATE AND NOT NECESSARILY COMPLETE. IT IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR TO INDEPENDENTLY VERIFY THE ACCURACY OF ALL UTILITY LOCATIONS.
7. THE CONTRACTOR SHALL LOCATE AND PROTECT ALL CASTINGS AND UTILITIES DURING CONSTRUCTION AND SHALL CONTACT THE UNDERGROUND UTILITIES LOCATE SERVICE (1-800-424-5555 OR 811) AT LEAST 48 HOURS PRIOR TO CONSTRUCTION.
8. INSPECTION AND ACCEPTANCE OF ALL WORK WILL BE ACCOMPLISHED BY REPRESENTATIVES OF THE CITY OF ARLINGTON. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO COORDINATE AND SCHEDULE APPROPRIATE INSPECTIONS, ALLOWING PROPER ADVANCE NOTICE. THE INSPECTOR MAY REQUIRE REMOVAL AND REPLACEMENT OF ITEMS THAT DO NOT MEET CITY STANDARDS OR WERE CONSTRUCTED WITHOUT INSPECTION.
9. THE CONTRACTOR SHALL KEEP THE ON-SITE AND OFF-SITE STREETS CLEAN AT ALL TIMES BY CLEANING WITH A SWEEPING AND/OR VACUUM TRUCK. WASHING OF THESE STREETS WILL NOT BE ALLOWED WITHOUT PRIOR APPROVAL FROM THE CITY INSPECTOR.
10. THE CONTRACTOR SHALL MAINTAIN TWO (2) SETS OF "AS- BUILT" PLANS SHOWING ALL FIELD CHANGES AND MODIFICATIONS. IMMEDIATELY AFTER CONSTRUCTION COMPLETION, THE CONTRACTOR SHALL DELIVER BOTH COPIES OF RED-LINED PLANS TO THE CITY. THE CITY WILL FORWARD ONE OF THE COPIES TO THE DESIGN ENGINEER.

ADDITIONAL GENERAL NOTES

1. THE CONTRACTOR AT THE TIME OF CONSTRUCTION IS REQUIRED TO SUPPLY MARKINGS FOR ANY TALL CONSTRUCTION EQUIPMENT SUCH AS CRANES THAT MAY BE USED. THE MARKINGS SUPPLIED SHOULD BE FLAGS, MINIMUM SIZE 3' x 3' IN DIMENSION WITH AN INTERNATIONAL ORANGE AND WHITE CHECKERBOARD PATTERN UTILIZING 1" x 1" SQUARES. THE FLAGS NEED TO BE PLACED ON THE HIGHEST PART OF THE EQUIPMENT TO SERVE AS A VISUAL WARNING TO PILOTS. ANY EQUIPMENT SUCH AS CRANES NEEDS TO BE LOWERED AT NIGHT.
2. REGARDING ADJACENT AIRCRAFT MOVEMENT AREAS, AT THE END OF THE WORKDAY THERE SHALL BE NO TEMPORARY OR PERMANENT PAVEMENT DROP OFFS GREATER THAN 1" OR TURF/GROUND LIPS GREATER THAN 3".
3. NO CONSTRUCTION EQUIPMENT/MATERIAL IS TO BE STORED BETWEEN THE BUILDING SITE AND THE EXISTING PAVED AREAS SERVING THE ADJACENT HANGARS.

WATER SYSTEM CONSTRUCTION NOTES

1. ALL WORK SHALL BE IN ACCORDANCE WITH THE APPROVED PLANS AND CURRENT EDITION OF THE CITY OF ARLINGTON STANDARDS AND SPECIFICATIONS. ANY CHANGES TO THE DESIGN REQUIRE CITY APPROVAL.
2. ALL MATERIALS SHALL CONFORM TO THE CITY STANDARDS AND SPECIFICATIONS AND SHALL BE APPROVED BY THE CITY PRIOR TO DELIVERY TO THE JOB SITE. MATERIAL SUBMITTALS ARE REQUIRED AND MUST BE APPROVED PRIOR TO SCHEDULING A PRE-CONSTRUCTION CONFERENCE.
3. WATER MAINS SHALL BE CEMENT-LINED DUCTILE IRON PIPE CLASS 52 UNLESS OTHERWISE APPROVED BY THE CITY.
4. ALL SERVICE LOCATIONS SHALL BE FIELD APPROVED BY THE CITY PRIOR TO INSTALLATION.
5. IT IS THE CONTRACTOR'S RESPONSIBILITY TO LOCATE UNDERGROUND UTILITIES BY CALLING ONE-CALL UNDERGROUND UTILITY LOCATOR (1-800-424-5555) 48 HOURS PRIOR TO CONSTRUCTION.
6. ADEQUATE TRENCH SHEETING AND/OR SHORING SHALL BE PROVIDED BY THE CONTRACTOR AS REQUIRED BY OSHA AND WSHA.
7. THE PIPE SHALL BE INSTALLED BY FOLLOWING THE FINISHED GRADE PROFILE WHEREVER POSSIBLE. THE DEPTH OF COVER SHALL BE TYPICALLY 36 INCHES (3 FEET), AND MAY BE UP TO 60 INCHES (5 FEET) WITH THE APPROVAL OF THE CITY ENGINEER. WATER MAINS UNDER THE STATE (OR COUNTY) HIGHWAYS SHALL MEET THE MINIMUM COVER DEPTHS REQUIRED BY WSDOT (OR SNOHOMISH COUNTY).
8. PIPE JOINT DEFLECTIONS SHALL NOT EXCEED ONE-HALF OF PIPE MANUFACTURER'S RECOMMENDED MAXIMUM DEFLECTIONS. BENDS MAY BE REQUIRED TO MAINTAIN PROPER WATER MAIN ALIGNMENT WITHIN EASEMENTS OR PUBLIC RIGHT-OF-WAY.
9. WATER MAIN SHALL NOT BE PLACED UNDER SIDEWALK, CURBS, GUTTERS, OR ANY PERMANENT STRUCTURES WITHOUT THE PRIOR APPROVAL OF THE CITY ENGINEER.
10. NO CONNECTION TO THE CITY WATER SYSTEM IS ALLOWED UNTIL THE NEW CONSTRUCTION PASSES PRESSURE AND PURITY TESTS, AND HAS BEEN APPROVED AND ACCEPTED BY THE CITY.
11. PRIOR TO MAKING CONNECTIONS TO THE EXISTING SYSTEM, CONTRACTOR SHALL FIELD VERIFY THE LOCATION, DEPTH AND MATERIAL OF EXISTING WATER MAINS AT THE POINT OF CONNECTION.
12. ALL WATER LINES SHALL BE A MINIMUM OF 10 FEET HORIZONTAL CLEARANCE FROM SANITARY SEWERS. WHEN WATER LINES CROSS SEWER LINES, THE MINIMUM VERTICAL CLEARANCE SHALL BE 18 INCHES.
13. TO FILL THE MAIN FOR FLUSHING, PRESSURE AND PURITY TESTS, THE CONTRACTOR IS REQUIRED TO USE AN APPROVED METER AND DCVA. THE HYDRANT SHALL REMAIN IN THE FULL OPEN POSITION TO PREVENT BACK-SIPHONAGE THROUGH THE DRAIN HOLE. AFTER SUCCESSFUL TESTING, THE CONTRACTOR WILL TIE INTO THE EXISTING SYSTEM, USING DISINFECTED SLEEVES AND SPOOL PIECES.
14. THE CONTRACTOR SHALL CONTACT THE CITY FOR VALVE OPERATION. ONLY AUTHORIZED REPRESENTATIVES OF THE CITY CAN OPERATE VALVES IN THE CITY WATER SYSTEM.
15. THE CONTRACTOR IS HEREBY NOTIFIED THAT, SINCE FILLING AND FLUSHING WILL BE DONE THROUGH A CROSS-CONNECTION CONTROL DEVICE, LOW VELOCITY FLOWS MAY BE ENCOUNTERED. THEREFORE, EVERY ATTEMPT SHALL BE MADE TO KEEP THE PIPE CLEAN DURING INSTALLATION. THIS MAY INCLUDE SWABING THE PIPE WITH CHLORINATED WATER.
16. THE CONTRACTOR SHALL PERFORM TEMPORARY SOIL EROSION AND SEDIMENTATION CONTROL, DUST CONTROL, NOISE CONTROL, AND TRAFFIC CONTROL AS REQUIRED BY THE CITY OR OTHER APPLICABLE AGENCIES.
17. CUTTING AND PATCHING OF ROAD WAYS SHALL CONFORM TO THE REQUIREMENTS OF THE RIGHT-OF-WAY PERMIT OF THE CITY OR OTHER APPLICABLE AGENCY.
18. INSTALL MEGA-LUGS AND THRUST BLOCKING ON ALL BENDS.

ESTIMATED GRADING QUANTITIES

CUT: 83 CU. YDS.
FILL: 197 CU. YDS.

QUANTITIES OF CUT AND FILL ARE APPROXIMATE AND ARE FOR THE PURPOSE OF OBTAINING A GRADING PERMIT. THEY ARE NOT INTENDED TO BE USED FOR CONTRACTUAL PURPOSES.

QUANTITIES CALCULATED BY AUTODESK LAND DESKTOP COMPOSITE METHOD.

FILL MATERIAL WILL BE COMPOSED OF NATIVE MATERIAL AND IMPORTED STRUCTURAL FILL. CRUSHED ROCK, CONCRETE AND ASPHALT WILL BE IMPORTED AND PLACED ON-SITE FOR THE CONSTRUCTION OF THE PAVED AREAS. ORGANIC MATERIAL MAY BE IMPORTED FOR CONSTRUCTION OF THE RAIN GARDENS

STORM DRAINAGE NOTES

1. ALL STORM DRAINAGE IMPROVEMENTS SHALL BE CONSTRUCTED IN ACCORDANCE WITH THESE APPROVED PLANS AND CITY STANDARDS AND SPECIFICATIONS. ANY DEVIATION FROM THESE PLANS WILL REQUIRE PRIOR APPROVAL FROM THE OWNER, THE CITY ENGINEER, AND OTHER APPROPRIATE PUBLIC AGENCIES.
2. ALL PIPE MATERIALS SHALL MEET THE REQUIREMENTS OF THE CITY STANDARDS AND SPECIFICATIONS. ACCEPTABLE STORM DRAINAGE PIPE MATERIALS INCLUDE CONCRETE, PVC, HDPE, AND DUCTILE IRON. CORRUGATED METAL PIPES (GALVANIZED ALUMINUM OR STEEL) ARE NOT ACCEPTED BY THE CITY. ALL PIPE JOINTS MUST HAVE GASKETS AND SHALL BE WATER TIGHT UNLESS OTHERWISE DIRECTED BY THE CITY.
3. PIPE BEDDING MATERIAL SHALL BE 5/8-INCH MINUS CRUSHED GRAVEL FOR ALL PIPE TYPES, EXCEPT DUCTILE IRON. BEDDING MATERIAL FOR DUCTILE IRON PIPE SHALL MEET THE REQUIREMENTS OF THE CITY'S STANDARDS AND SPECIFICATIONS (CHAPTER 4).
4. ALL TRENCH BACKFILL IN AREAS OF PAVEMENT OR STRUCTURAL LOADING SHALL BE COMPACTED TO AT LEAST 95% OF THE MAXIMUM DRY DENSITY. ALL OTHER AREAS SHALL BE COMPACTED TO AT LEAST 90% OF MAXIMUM DRY DENSITY.
5. ALL PIPE SHALL BE PLACED ON STABLE EARTH. IF IN THE OPINION OF THE CITY INSPECTOR, THE EXISTING TRENCH FOUNDATION IS UNSATISFACTORY, THEN IT SHALL BE EXCAVATED BELOW GRADE AND BACKFILLED WITH GRAVEL BEDDING MATERIAL TO SUPPORT THE PIPE.
6. LOT DRAINAGE SYSTEMS, STUB-OUTS AND ANY DRAINS BEHIND THE SIDEWALK MUST BE INSTALLED AS REQUIRED PRIOR TO SIDEWALK CONSTRUCTION. STUB-OUTS SHALL BE MARKED WITH A 2"x4" WITH 3 FEET VISIBLE ABOVE GRADE AND MARKED "STORM". LOCATION AND DEPTH OF THESE INSTALLATIONS SHALL BE SHOWN ON THE AS-BUILT PLANS SUBMITTED TO THE CITY.
6. ALL CATCH BASINS SHALL BE TYPE 1 UNLESS OTHERWISE SHOWN ON THE PLANS AND APPROVED BY THE CITY. THE USE AND INSTALLATION OF INLETS IS DISCOURAGED.
7. ALL CATCH BASINS WITH A DEPTH OF 5 FEET (RIM TO INVERT) OR GREATER SHALL BE TYPE 2 CATCH BASINS EQUIPPED WITH 3/4-INCH DIAMETER SAFETY MANHOLE STEPS OR A MANHOLE LADDER PER CITY STANDARD DETAILS.
8. ALL GRATES SHALL BE MARKED "OUTFALL TO STREAM - DUMP NO POLLUTANTS". ALL SOLID COVER SHALL BE MARKED "DRAIN". ALL CATCH BASINS AND MANHOLES SHALL BE EQUIPPED WITH LOCKING FRAMES AND LIDS OR GRATES PER CITY STANDARD DETAILS.
9. ALL GRATES LOCATED IN THE GUTTER FLOW LINE (INLET AND CATCH BASIN) SHALL BE DEPRESSED 0.1 FOOT BELOW PAVEMENT LEVEL.
10. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ADJUSTING ALL MANHOLE, INLET AND CATCH BASIN FRAMES AND GRATES/COVERS TO GRADE JUST PRIOR TO CURB INSTALLATION AND/PAVING.
11. ALL RETENTION/DETENTION FACILITIES SHALL BE INSTALLED AND IN OPERATION PRIOR TO, OR IN CONJUNCTION WITH, ALL CONSTRUCTION ACTIVITY, UNLESS OTHERWISE APPROVED BY THE CITY.
12. DETENTION/RETENTION PONDS WITH SIDE SLOPES STEEPER THAN 3:1 OR WITH A MAXIMUM WATER DEPTH GREATER THAN 3 FEET SHALL BE ENCLOSED WITH A VINYL COATED CHAIN LINK FENCE.
12. BIO-FILTRATION SWALES AND/OR FILTER STRIPS SHALL BE CONSTRUCTED, BEDDED OR SEEDED AND IN OPERATION PRIOR TO, OR SODDED IN CONJUNCTION WITH, ASPHALT PAVING. THE VEGETATION IN THE BIO-SWALE MUST BE WELL ESTABLISHED BEFORE PAVING BEGINS.
13. STORM WATER RETENTION/DETENTION FACILITIES, STORM DRAINAGE PIPE AND CATCH BASINS SHALL BE FLUSHED AND CLEANED BY THE DEVELOPER PRIOR TO THE CITY'S ACCEPTANCE OF THE PROJECT.
14. WHEN INFILTRATION FACILITIES ARE CONSTRUCTED, COMPACTION OF SOIL IS NOT ALLOWED, AS THE DESIGN IS BASED ON NATURAL SOIL IN THE ORIGINAL LOCATION. VEHICLES SHALL NOT BE DRIVEN OVER THE INFILTRATION AREA DURING CONSTRUCTION.
15. IF THE CONTRACTOR ENCOUNTERS GROUNDWATER OR SOIL CONDITIONS DIFFERENT FROM THAT SHOWN IN THE PLANS DURING INFILTRATION SYSTEM INSTALLATION, THE CONTRACTOR SHALL NOTIFY THE CITY INSPECTOR.

TESC NOTES

1. APPROVAL OF THE TEMPORARY EROSION/SEDIMENT CONTROL (TESC) PLAN DOES NOT CONSTITUTE AN APPROVAL OF PERMANENT ROAD OR STORM DRAINAGE DESIGN.
2. A TESC PLAN MEETING THE DOE STORM WATER MANAGEMENT MANUAL ADOPTED BY THE CITY SHALL BE SUBMITTED TO THE CITY FOR APPROVAL PRIOR TO ANY WORK ON THE SITE. AN APPROVED COPY MUST BE MAINTAINED ON-SITE AND BE READILY AVAILABLE TO THE CITY INSPECTOR AT THEIR REQUEST.
3. THE TESC BMP'S SHOWN ON THE PLAN MUST BE INSTALLED PRIOR TO ALL OTHER CLEARING AND GRADING ACTIVITIES. THE CONTRACTOR SHALL ENSURE THAT SEDIMENT-LADEN WATER DOES NOT ENTER THE DRAINAGE SYSTEM, LEAVE THE SITE, OR VIOLATE APPLICABLE WATER QUALITY STANDARDS. MAINTENANCE, REPLACEMENT, AND UPGRADING OF THE TESC PLAN IS THE RESPONSIBILITY OF THE CONTRACTOR UNTIL ALL CONSTRUCTION IS COMPLETE AND APPROVED BY THE CITY.
4. THE BOUNDARIES OF THE CLEARING LIMITS, SHOWN ON THE TESC PLAN, SHALL BE CLEARLY FENCED OR FLAGGED IN THE FIELD PRIOR TO STARTING CONSTRUCTION. NO DISTURBANCE BEYOND THE FENCED OR FLAGGED CLEARING LIMITS SHALL BE PERMITTED. THE FENCING AND/OR FLAGGING SHALL BE MAINTAINED BY THE CONTRACTOR FOR THE DURATION OF THE CONSTRUCTION PROJECT.
5. THE TESC FACILITIES SHOWN ON THE PLANS ARE THE MINIMUM REQUIREMENTS FOR THE ANTICIPATED SITE CONSTRUCTION. DURING THE CONSTRUCTION PERIOD, THESE TESC FACILITIES SHALL BE UPGRADED AND ADDED TO AS NEEDED, FOR UNEXPECTED STORM EVENTS AND TO REFLECT CHANGING CONDITIONS, AS REQUIRED BY THE CITY.
6. THE CONTRACTOR SHALL PROVIDE THE CITY A 24-HOUR EMERGENCY CONTACT PHONE NUMBER OF THE CONTRACTOR'S CERTIFIED EROSION CONTROL SUPERVISOR PRIOR TO STARTING CONSTRUCTION.
7. THE TESC FACILITIES SHALL BE INSPECTED DAILY BY THE CONTRACTOR AND MAINTAINED AS NECESSARY TO ENSURE CONTINUED FUNCTION AND OPERATION.
8. BETWEEN OCTOBER 1 AND APRIL 30, DISTURBED AREAS THAT ARE TO BE LEFT UNWORKED FOR MORE THAN TWO (2) DAYS SHALL BE IMMEDIATELY COVERED BY MULCH, SOD OR PLASTIC COVERING. BETWEEN MAY 1 AND SEPTEMBER 30, DISTURBED AREAS THAT ARE TO BE LEFT UNWORKED FOR MORE THAN SEVEN (7) DAYS SHALL BE IMMEDIATELY COVERED BY SEEDING OR OTHER APPROVED METHODS.
9. SEDIMENT DEPOSITS SHALL BE REMOVED FROM ALL CATCH BASINS, PRE-TREATMENT/SEDIMENT POND, AND SEDIMENT TRAPS UPON REACHING A DEPTH OF 12 INCHES.
10. ANY PERMANENT RETENTION/DETENTION FACILITY USED AS A TEMPORARY SETTLING BASIN SHALL BE MODIFIED WITH THE NECESSARY EROSION CONTROL MEASURES, SHALL PROVIDE ADEQUATE STORAGE CAPACITY, AND SHALL BE CLEANED OUT ENTIRELY ONCE THE SITE IS STABILIZED. IF THE PERMANENT FACILITY IS TO ULTIMATELY FUNCTION AS AN INFILTRATION SYSTEM, THE FACILITY SHALL NOT BE USED AS A TEMPORARY SETTLING BASIN.
11. WHERE SEEDING FOR TEMPORARY EROSION CONTROL IS REQUIRED, FAST GERMINATING GRASSES SHALL BE APPLIED AT AN APPROXIMATE RATE OF 120 LBS PER ACRE.
12. WHERE STRAW MULCH FOR TEMPORARY EROSION CONTROL IS REQUIRED, IT SHALL BE APPLIED AT A MINIMUM THICKNESS OF 3 INCHES, OR 3,000 POUNDS PER ACRE.
13. SOIL STOCKPILES SHALL BE STABILIZED WITHIN 24 HOURS. WHEN ACTIVELY WORKING WITH THE SOIL STOCKPILE, STABILIZATION BY GROUND COVER BMPs SHALL OCCUR AT THE END OF EACH WORK DAY.
14. STABILIZED CONSTRUCTION ENTRANCES SHALL BE INSTALLED AT THE BEGINNING OF CONSTRUCTION AND MAINTAINED FOR THE DURATION OF THE PROJECT. ADDITIONAL MEASURES MAY BE REQUIRED TO INSURE THAT ALL PAVED AREAS ARE KEPT CLEAN FOR THE DURATION OF THE PROJECT.
15. MAINTENANCE AND REPAIR OF TESC FACILITIES AND STRUCTURES SHALL BE CONDUCTED IMMEDIATELY UPON RECOGNITION OF A PROBLEM OR WHEN THE TESC MEASURES BECOME DAMAGED.
16. UPON COMPLETION OF THE PROJECT, ALL BMP'S SHALL BE REMOVED FROM THE SITE AND RIGHT OF WAY. IF BMP'S ARE REQUIRED TO REMAIN IN PLACE FOR FURTHER PROTECTION, ARRANGEMENTS FOR REMOVAL SHALL BE MADE WITH THE CITY INSPECTOR.
17. ALL INFILTRATION FACILITIES SHALL BE PROTECTED FROM ALL COMPACTION BY CONSTRUCTION EQUIPMENT.

SITE GRADING NOTES

1. ALL GRADING SHALL COMPLY WITH CHAPTER A33 OF THE UNIFORM BUILDING CODE (CURRENT EDITION), AND APPLICABLE CITY OF ARLINGTON CODES.
2. PUBLIC STREETS AND TAXILANES / TAXIWAYS ARE TO BE KEPT CLEAR OF DIRT AND DEBRIS DURING EXCAVATION AND FILL OPERATIONS.
3. AREAS TO RECEIVE FILL SHALL BE CLEARED OF ALL VEGETATION AND DELETERIOUS MATTER.
4. AREAS TO RECEIVE FILL SHALL BE PROOF ROLLED. ALL LOOSE AND SOFT AREAS SHALL BE REMOVED AND REPLACED WITH STRUCTURAL FILL.
5. ALL FILL MATERIALS USED SHALL BE FREE OF VEGETATION AND DELETERIOUS MATTER AND SHALL NOT CONTAIN ROCKS GREATER THAN SIX INCHES IN DIAMETER.
6. STRUCTURAL FILLS SHALL BE PLACED IN EIGHT TO 10 INCH THICK LOOSE HORIZONTAL LIFTS AND SPREAD UNIFORMLY.
7. AFTER EACH LIFT HAS BEEN SPREAD EVENLY, IT SHALL BE COMPACTED TO A MINIMUM OF 95% OF MAXIMUM DENSITY DETERMINED IN ACCORDANCE WITH ASTM D1557 (MODIFIED PROCTOR).
8. THE SURFACE OF ALL SLOPES SHALL BE COMPACTED. THIS MAY BE ACCOMPLISHED BY OVER-BUILDING THE SLOPES, THEN CUTTING BACK TO FINAL GRADES, OR BY RUNNING THE COMPACTOR OVER THE SLOPE AS EACH FILL LIFT IS BEING PLACED. ALL SLOPES SHALL BE COMPACTED BY THE END OF EACH WORK DAY.
9. FIELD DENSITY TESTS (IF REQUIRED) WILL BE MADE BY A QUALIFIED SOILS ENGINEERING FIRM. DENSITY TESTS SHALL BE TAKEN AT OR JUST BELOW THE SURFACE AT THE FREQUENCY AND LOCATIONS DETERMINED BY THE SOILS ENGINEERING FIRM. WHEN THE TESTS INDICATE THAT THE DENSITY OF ANY LAYER OF FILL OR PORTION THEREOF IS BELOW THE SPECIFIED DENSITY, THE PARTICULAR SECTION SHALL BE REMORKED UNTIL THE REQUIRED DENSITY HAS BEEN OBTAINED.
10. ALL CUT AND FILL SLOPES SHALL BE A MAXIMUM OF 2:1.
11. STOCKPILES ARE TO BE LOCATED IN SAFE AREAS AND ADEQUATELY PROTECTED TO PREVENT EROSION. HYDROSEED PREFERRED.
12. CONTRACTOR SHALL MAINTAIN A MINIMUM 2-FT GRADING SETBACK FROM PROPERTY BOUNDARY UNLESS WRITTEN PERMISSION IS OBTAINED FROM ADJACENT PROPERTY OWNER.
13. EXCESS MATERIAL MAY BE SPREAD EVENLY ACROSS SITE, PROVIDED IT DOES NOT INTERFERE WITH OR HINDER THE DRAINAGE PATTERN ESTABLISHED ON THE GRADING PLANS.
14. CONTRACTOR SHALL MAINTAIN A MINIMUM SLOPE OF 0.5% ACROSS AT ALL TIMES.

SEWER SYSTEM CONSTRUCTION NOTES

1. ALL WORK SHALL BE IN ACCORDANCE WITH THE APPROVED PLANS, AND CURRENT EDITION OF THE CITY OF ARLINGTON STANDARDS AND SPECIFICATIONS. ANY CHANGES TO THE DESIGN REQUIRES CITY APPROVAL.
2. ALL MATERIALS SHALL CONFORM TO THE CITY OF ARLINGTON STANDARDS AND SPECIFICATIONS AND THE MATERIAL SUBMITTALS SHALL BE APPROVED BY THE CITY BEFORE SCHEDULING THE PRECONSTRUCTION CONFERENCE AND BEFORE THE MATERIALS ARE DELIVERED TO THE JOB SITE. ONCE THE MATERIALS ARE DELIVERED TO THE JOB SITE, THE INSPECTOR WILL DETERMINE IF THE MATERIALS WERE MANUFACTURED TO MEET THE REQUIREMENTS OF THE CITY OF ARLINGTON STANDARDS AND SPECIFICATIONS BEFORE THE MATERIALS CAN BE INSTALLED.
3. ALL SEWER MAINS AND SIDE SEWER STUBS SHALL BE FIELD STAKED FOR GRADES AND ALIGNMENT BY A SURVEYOR PRIOR TO CONSTRUCTION. THE CONSTRUCTION STAKES MUST SHOW THE STATION AND OFFSET TO THE ALIGNMENT.
4. THE CITY OF ARLINGTON WASTEWATER DIVISION SHALL BE NOTIFIED A MINIMUM OF 48 HOURS IN ADVANCE OF A TAP OR CONNECTION TO AN EXISTING SANITARY SEWER MAIN. THE INSPECTOR SHALL BE PRESENT AT THE TIME OF THE TAP OR CONNECTION.
5. GRAVITY SEWERS, INCLUDING SIDE SEWERS, WITH 5 TO 14 FEET OF COVER SHALL BE PVC ASTM D 3034 SDR 35. GRAVITY SEWER MAINS WITH LESS THAN 5 FEET OR GREATER THAN 14 FEET OF COVER SHALL BE DUCTILE IRON PIPE CLASS 52, OR C-900 PVC. IF DUCTILE IRON PIPE IS USED FOR SEWER, THE PIPE INTERIOR SHALL BE EPOXY COATED (NOT CEMENT-LINED).
6. PRE-CAST MANHOLES SHALL MEET THE REQUIREMENTS OF ASTM C-478. JOINTS SHALL BE RUBBER GASKETED AND GROUTED BOTH INSIDE AND OUTSIDE OF THE MANHOLE PER CITY OF ARLINGTON STANDARDS AND SPECIFICATIONS. ALL LIFT HOLES CUT THROUGH THE WALLS OF THE MANHOLE SHALL BE GROUTED FROM THE INSIDE AND OUTSIDE OF THE MANHOLE TO BE WATERTIGHT. ADDITIONAL WATERPROOFING MAY BE REQUIRED.
7. SIDE SEWER SERVICES SHALL BE PVC ASTM D 3034 SDR 35 WITH FLEXIBLE GASKETED JOINTS. SIDE SEWER CONNECTIONS SHALL BE MADE BY A TAP TO AN EXISTING MAIN OR A TEE FROM A NEW MAIN CONNECTED ABOVE THE SPRING LINE OF THE PIPE. WYES ARE NOT ALLOWED ON LINES 8" OR LARGER. SIDE SEWERS CAN NOT BE INSTALLED UNDER DRIVEWAYS UNLESS APPROVED BY THE CITY INSPECTOR.
8. ALL SEWER PIPE SHALL BE INSTALLED WITH A CONTINUOUS TRACER TAPE 24"-48" UNDER THE PROPOSED FINISHED SUBGRADE, OR AS DIRECTED BY THE CITY INSPECTOR. THE MARKER SHALL BE PLASTIC, NON-Biodegradable, METAL CORE, AND DETECTABLE, WITH BACKING MARKED "SEWER".
9. SIDE SEWERS SHALL BE INSTALLED BY THE DEVELOPER AND COORDINATED FOR CLEARANCE WITH POWER, GAS, TELEPHONE, CABLE, AND OTHER UTILITIES. SIDE SEWERS SHALL BE A MINIMUM OF 10 FEET BEYOND PROPERTY LINES AND 5' BEYOND ANY EASEMENT.
10. THE SEWER PIPE SHALL BE INSTALLED STARTING FROM DOWNSTREAM OF THE POINT OF CONNECTION ON THE EXISTING SEWER OR FROM A DESIGNATED STARTING POINT. THE SEWER PIPE SHALL BE INSTALLED WITH THE BELL END UPSTREAM.
11. ADEQUATE TRENCH SHEETING AND/OR SHORING SHALL BE PROVIDED BY THE CONTRACTOR AS REQUIRED BY OSHA AND WSHA.
12. TO PREVENT WATER OR DEBRIS FROM DISCHARGING INTO THE CITY'S EXISTING SEWER SYSTEM, THE CONTRACTOR SHALL INSTALL A PLUG IN THE CONNECTION MANHOLE OR AS DIRECTED BY THE CITY INSPECTOR. THE PLUG SHALL NOT BE REMOVED UNTIL THE SEWER IS ACCEPTED BY THE CITY.
13. ALL SEWER LINES SHALL MAINTAIN A MINIMUM OF 10 FEET HORIZONTAL CLEARANCE AND A MINIMUM OF 18 INCHES VERTICAL CLEARANCE FROM WATER LINES. SEE STANDARDS AND SPECIFICATIONS FOR ADDITIONAL REQUIREMENTS AND ALTERNATIVES.
14. PIPE BEDDING SHALL BE IN ACCORDANCE WITH THE CITY OF ARLINGTON STANDARD AND WSDOT STANDARD SPECIFICATIONS. 3/8-INCH MINUS MANUFACTURED CLEAN PEA GRAVEL IS THE REQUIRED BEDDING MATERIAL. ALL PIPE SHALL BE LAID ON A PROPERLY PREPARED FOUNDATION. THIS SHALL INCLUDE NECESSARY LEVELING OF THE TRENCH BOTTOM OR THE TOP OF THE FOUNDATION MATERIALS AS WELL AS PLACEMENT AND COMPACTION OF REQUIRED BEDDING MATERIAL TO UNIFORM GRADE SO THAT THE ENTIRE LENGTH OF THE PIPE WILL BE SUPPORTED BY A UNIFORMLY DENSE UNYIELDING BASE.
15. THE CONTRACTOR SHALL COMPACT TRENCH BACKFILL WITHIN THE CITY RIGHT-OF-WAY TO AT LEAST 90% MAXIMUM DRY DENSITY FROM THE BOTTOM OF THE TRENCH TO A DEPTH OF 3' BELOW THE SURFACE. THE TRENCH BACKFILL MUST BE COMPACTED TO AT LEAST 95% MAXIMUM DENSITY WITHIN 3' OF THE SURFACE. ASPHALT MUST BE COMPACTED TO MEET THE REQUIREMENTS ON THE PLANS. ALL COMPACTION TESTS ARE AT THE DEVELOPER'S EXPENSE.

ADA NOTES

THE FOLLOWING NOTES SHALL APPLY TO HARDSCAPE FACILITIES WITHIN THE PUBLIC RIGHT-OF-WAY.

1. UNLESS OTHERWISE SPECIFIED ON THE PLANS, OR DIRECTED BY THE CITY ENGINEER, CROSS SLOPES OF SIDEWALKS SHALL BE NOMINAL 1.5%. CROSS SLOPES SHALL NOT BE LESS THAN 1% NOR EXCEED 2%.
2. MAXIMUM SLOPES SHOWN ON THE PLANS REPRESENT THE MAXIMUM ALLOWABLE SLOPES PERMITTED BY CURRENT ADA REQUIREMENTS. THE CONTRACTOR SHALL TAKE INTO CONSIDERATION CONSTRUCTION TOLERANCES WHEN PLACING SIDEWALKS TO INSURE MAXIMUM SLOPES ARE NOT EXCEEDED.
3. COMPLETED SIDEWALKS OR OTHER HARDSCAPE ELEMENTS THAT EXCEED MAXIMUM SPECIFIED SLOPES OR ARE LESS THAN MINIMUM SPECIFIED SLOPES SHALL BE REMOVED AND REPLACED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.

LEGEND

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DESIGNED BY:		DRAWN BY:	
KSH		KSH/USM	

●	PROP. FIRE HYDRANT
⌘	EX. WATER VALVE
○	EX. SANITARY SEWER MANHOLE
⊙	PROP. SANITARY SEWER MANHOLE
•	PROP. SANITARY SEWER CLEAN-OUT
□	EX. STORM CATCH BASIN
▣	PROP. STORM CATCH BASIN
■	PROP. YARD DRAIN
⊗	PROP. OBSERVATION WELL
■	PROP. WATER METER
⊕	EX. STORM OVERFLOW
⊞	EX. POWER VAULT
○	EX. POWER LOCATE PAINT
●	EX. WATER LOCATE PAINT
●	EX. GAS LOCATE PAINT
✱	PROP. LUMINAIRE
⊙	PROP. SPOT ELEVATION
⊙	PROP. ORNAMENTAL TREE
⊙	PROP. GRASS
⊙	EXISTING ASPHALT
⊙	PROPOSED ASPHALT
—SS—	SANITARY SEWER LINE
—SD—	STORM DRAIN SEWER LINE
—CHP—	OVERHEAD POWER LINE
—UGP—	UNDERGROUND POWER LINE
—UT—	UNDERGROUND T.V. CABLE
—G—	GAS LINE
—T—	UNDERGROUND TELEPHONE LINE
—X—	FENCE

AS-BUILT DRAWING APPROVAL
ACKNOWLEDGMENT

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BY:

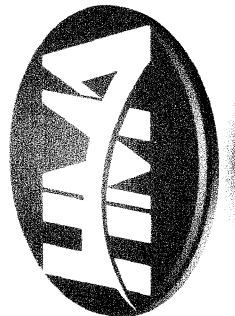
City Engineer, City of Arlington

DATE:

01-29-14

HALE MILLIGAN & ASSOCIATES
ENGINEERING

307 N Olympic Avenue Suite 209 Arlington, VA 98023
P: 360.474.4624 F: 425.968.1245

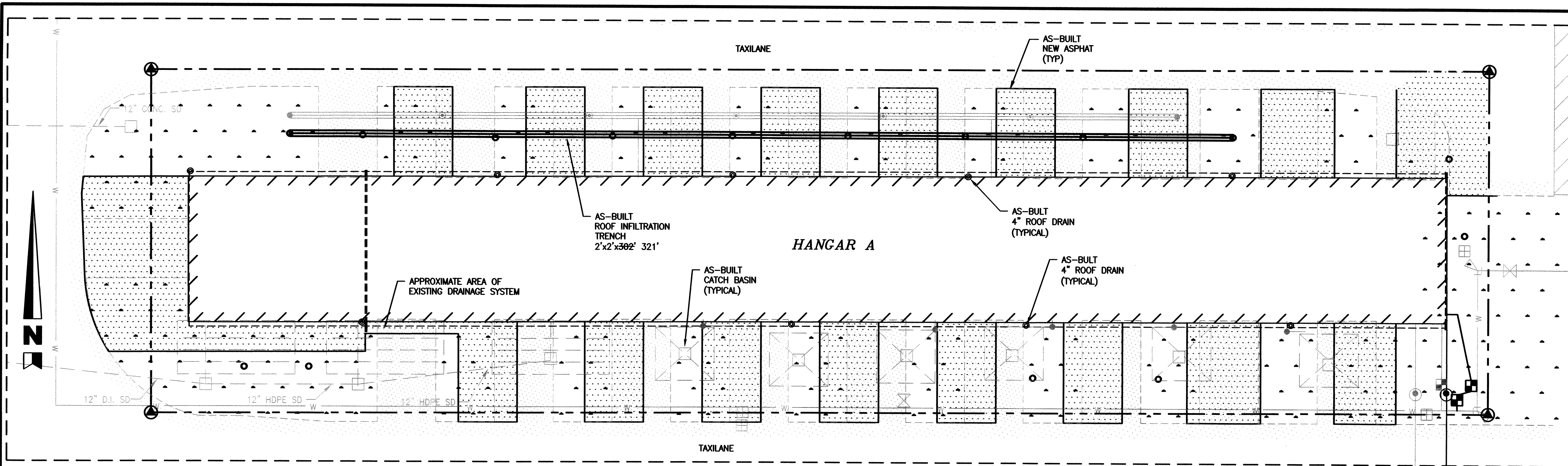


CARRERA PACIFIC HANGARS
ARLINGTON AIRPORT LOT 12 & 13

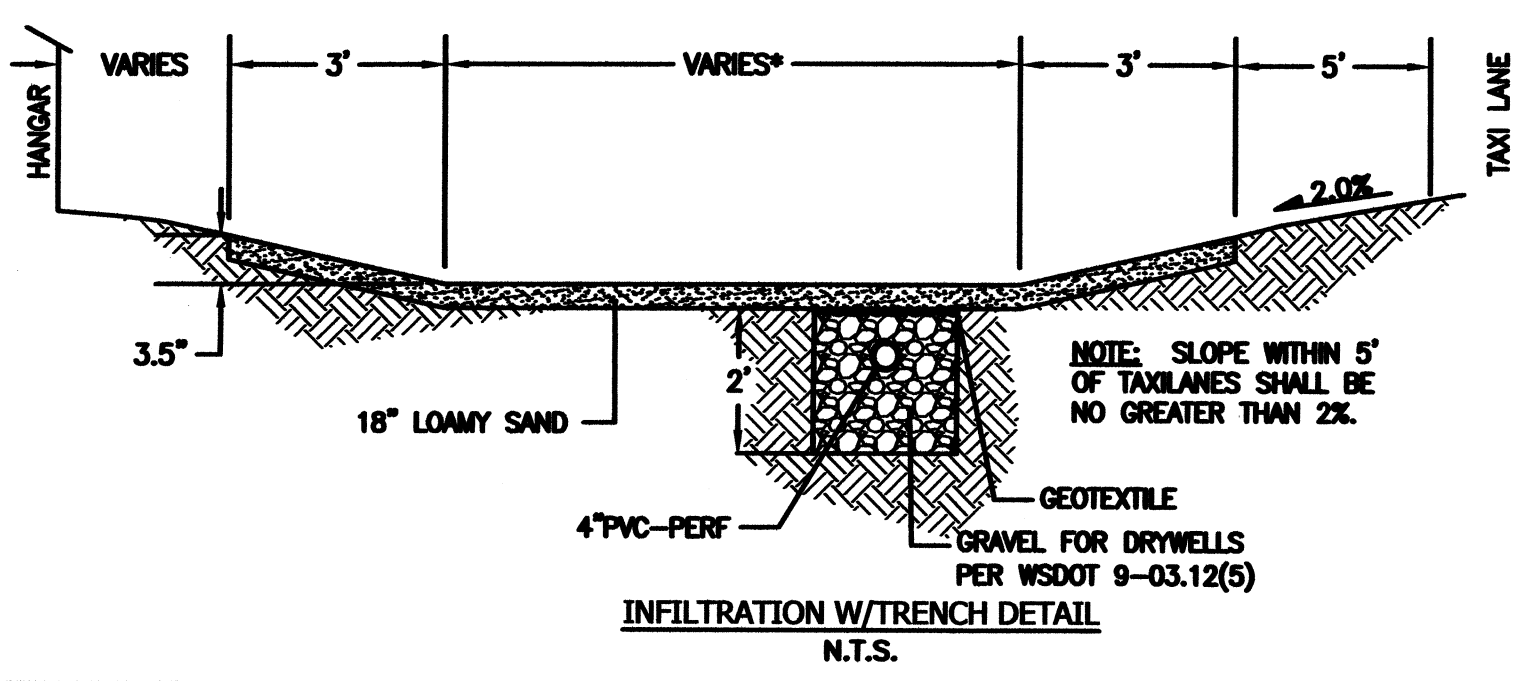
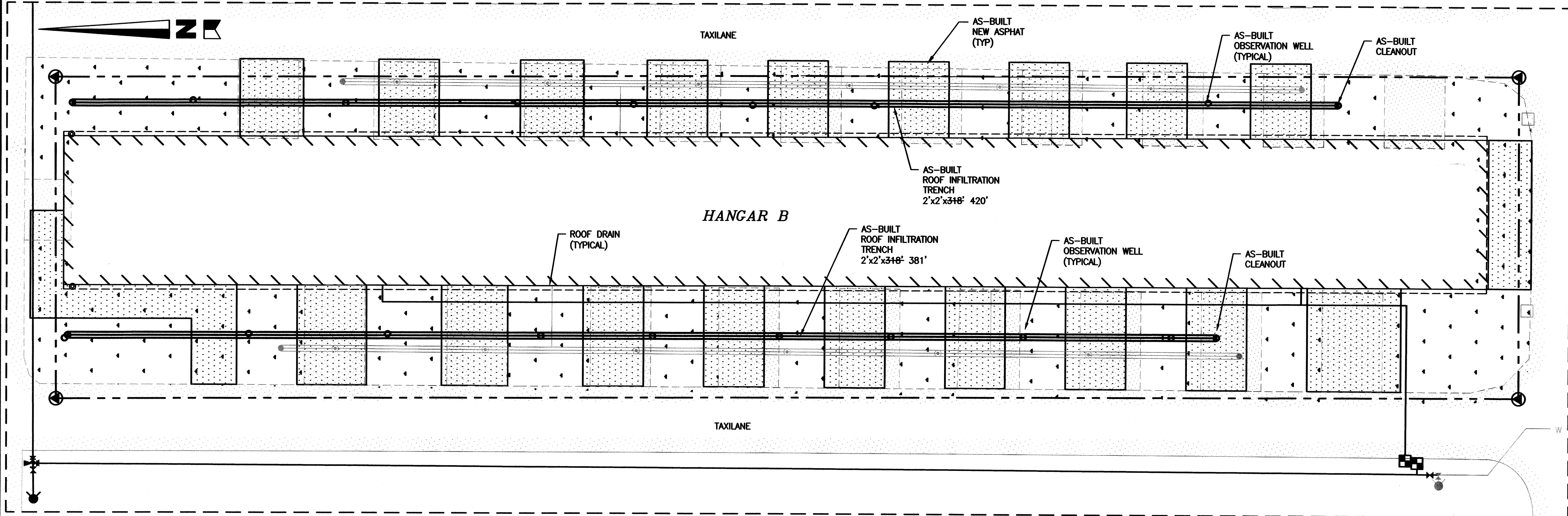
GENERAL NOTES

20110017

2 OF 7



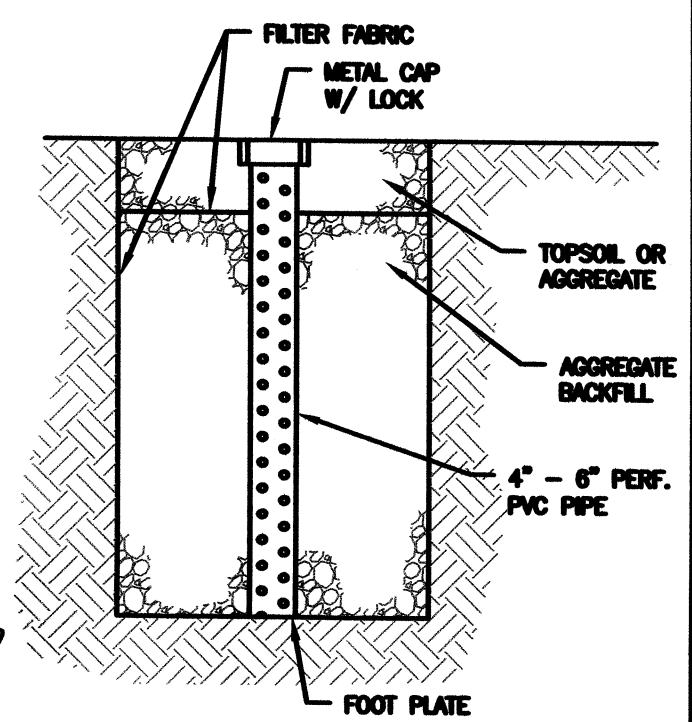
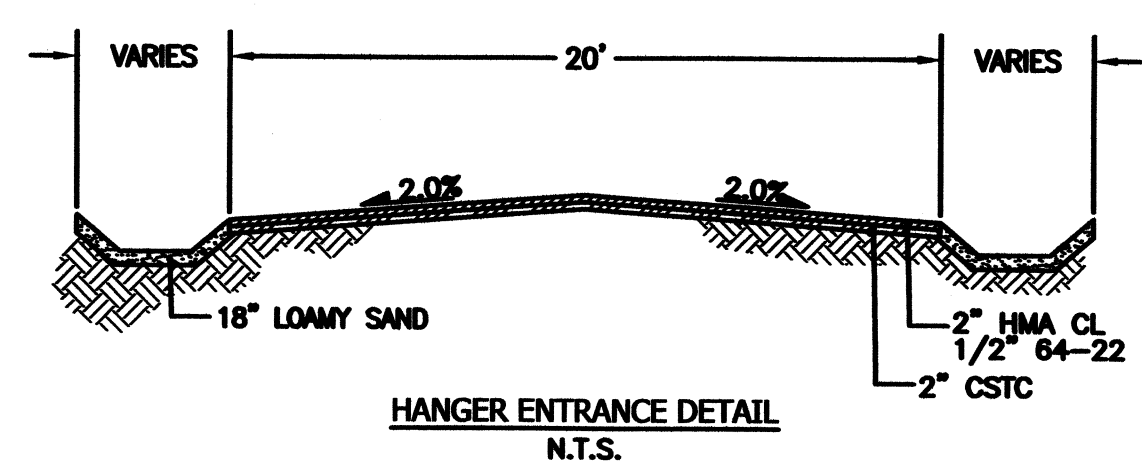
NOTES:
1. INFILTRATION TRENCH SIZES ARE ESTIMATED BASED ON SURFACE FEATURES. ENGINEER WAS UNABLE TO VERIFY LENGTHS DURING INSTALLATION.



BASIN DIMENSIONS:

BASINS:	A NORTH	B WEST	B EAST
LENGTH	16	20	14
WIDTH	14	14	14
100-YR	3.0'	2.75'	2.7'

INFLTRATION W/O TRENCH DETAIL
N.T.S.



OBSERVATION WELL DETAIL
N.T.S.
NOTE: INSTALL OBSERVATION WELLS EVERY 50' OF TRENCH LENGTH PER DETAIL

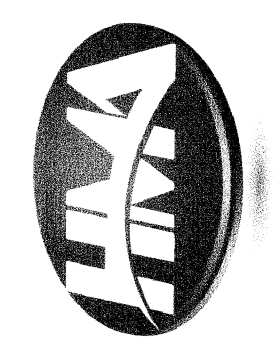
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ACKNOWLEDGMENT

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BY: *[Signature]*
City Engineer, City of Arlington

DATE: 01-29-14

HALE MILLIGAN & ASSOCIATES
ENGINEERING
307 N Olympic Avenue Suite 209 Arlington, WA 98223
P-360-474 4624 F-425 966 1245

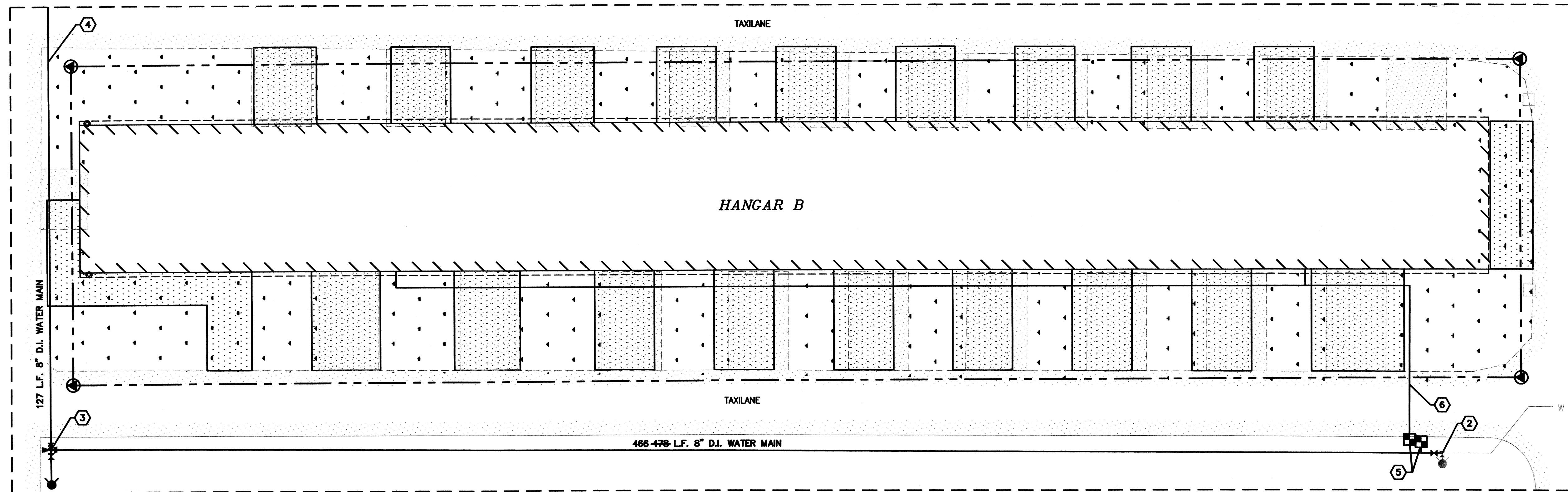
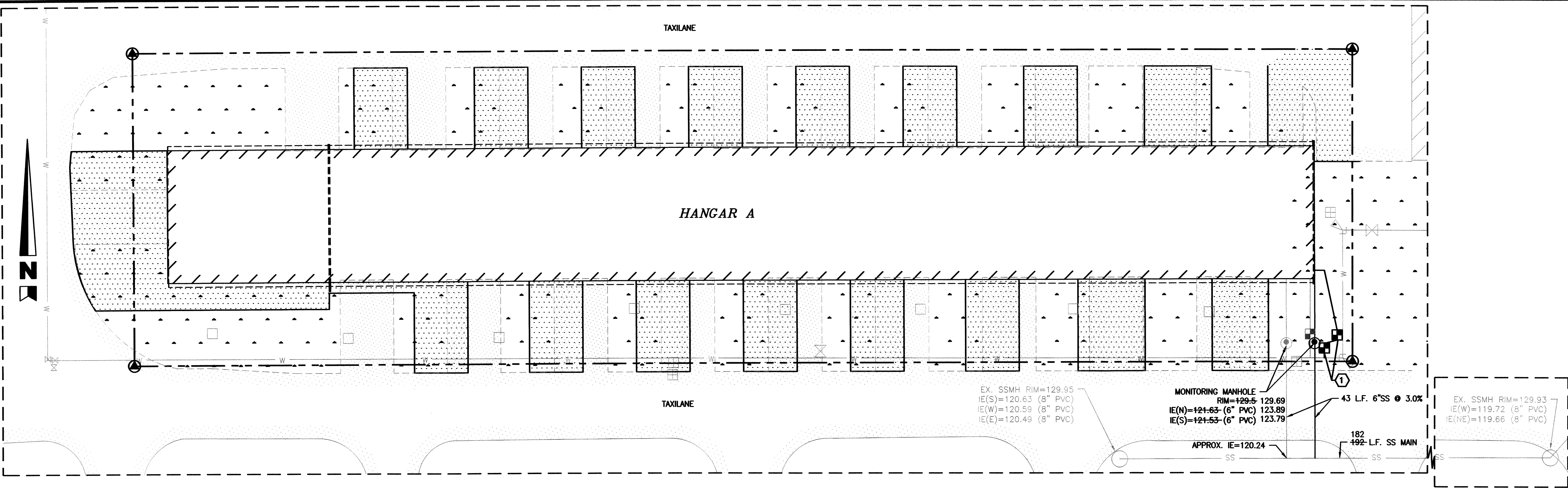


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07/05/12	3	CITY COMMENTS	KSH
12/28/11	2	CITY COMMENTS	KSH
12/12/11	1	CITY COMMENTS	KSH

DESIGNED BY: KSH
DRAWN BY: KSH/JSM

CARRERA PACIFIC HANGARS
ARLINGTON AIRPORT LOT 12 & 13
GRADING & DRAINAGE PLAN

20110017



WATER COSTRUCTION NOTES

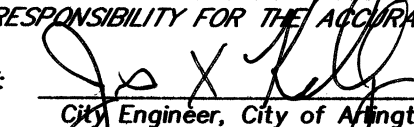
- ① 1 - 5/8" WATER METER
1 - DCVA
- ② 1 - 12" X 8" REDUCER (M&FL)
1 - 8" G.V. (FL&MJ)
- ③ 1 - FIRE HYDRANT ASSEMBLY
1 - 8" TEE (FL)
2 - 8" G.V. (FL&MJ)
1 - 6" G.V. (FL&MJ)
1 - 8" X 6" REDUCER (FL)
1 - THRUST BLOCK
- ④ EXTENDED BY CITY OF ARLINGTON
- ⑤ 1 - 5/8" WATER METER
1 - DCVA
- ⑥ 2" SERVICE LINE

LEGEND

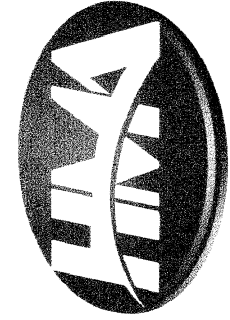
- FIRE HYDRANT
○ WATER METER
○ X WATER VALVE
○ SANITARY SEWER CLEAN-OUT
○ SANITARY SEWER MANHOLE

AS-BUILT DRAWING APPROVAL
ACKNOWLEDGMENT

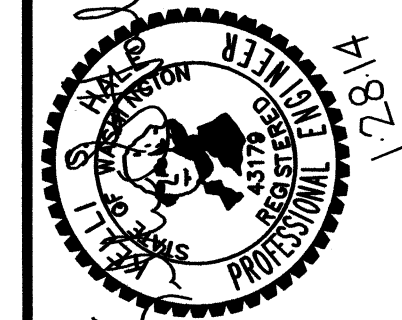
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BY: 
City Engineer, City of Arlington
DATE: 01-29-14

HALE MILLIGAN & ASSOCIATES
ENGINEERING
307 N Olympic Avenue Suite 203 Arlington, WA 98223
P:360.474.4624 F:425.968.1245



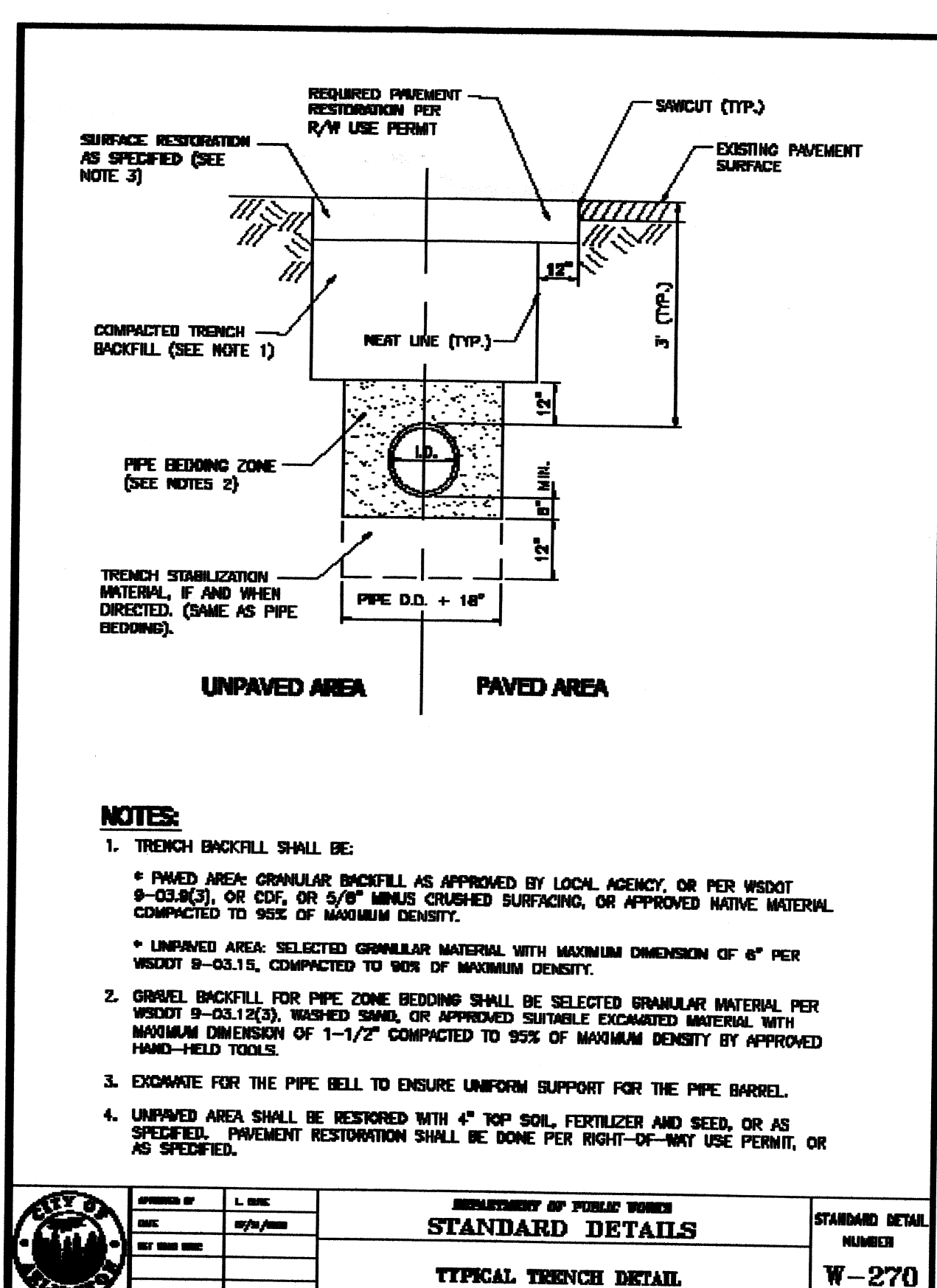
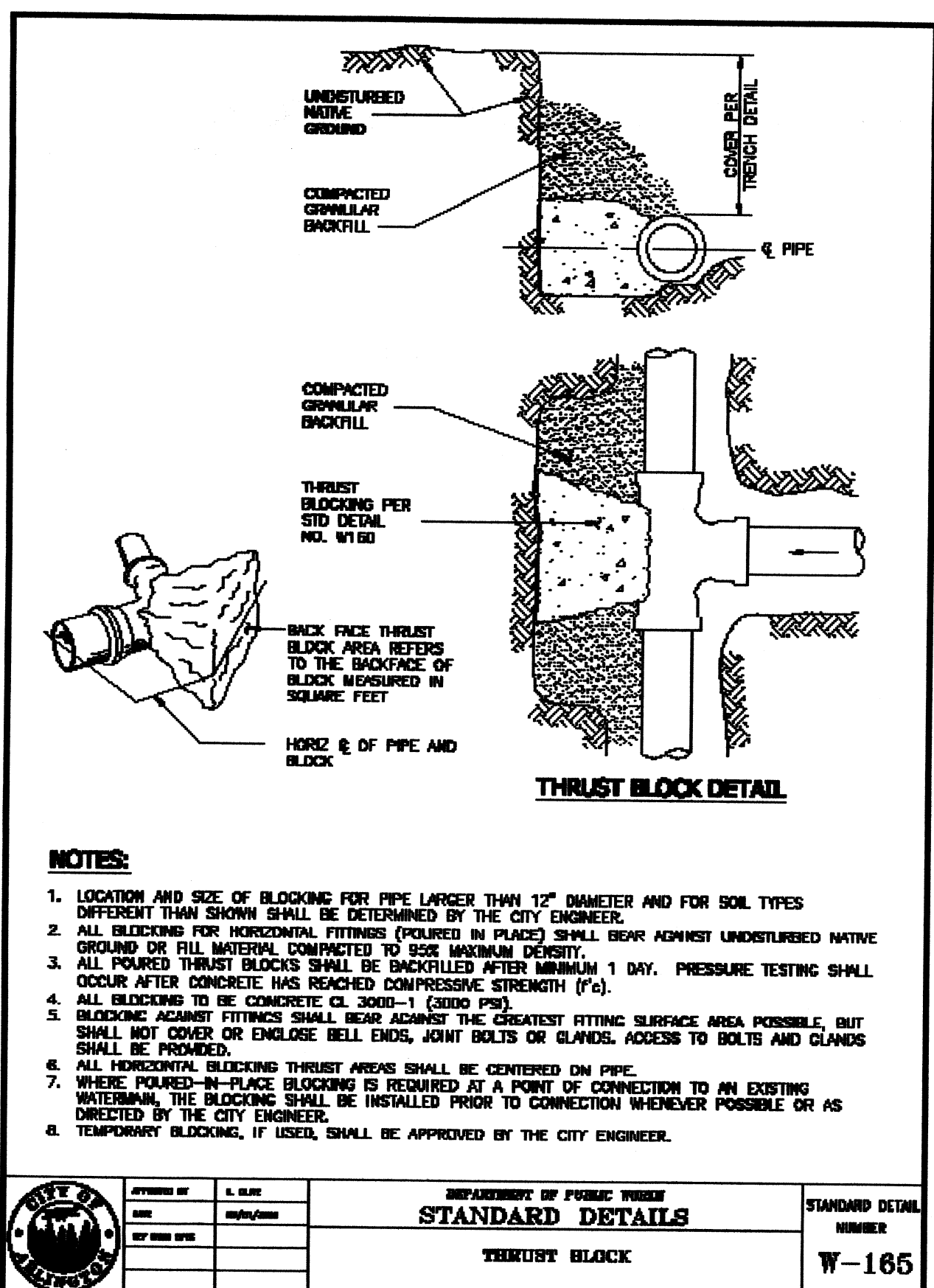
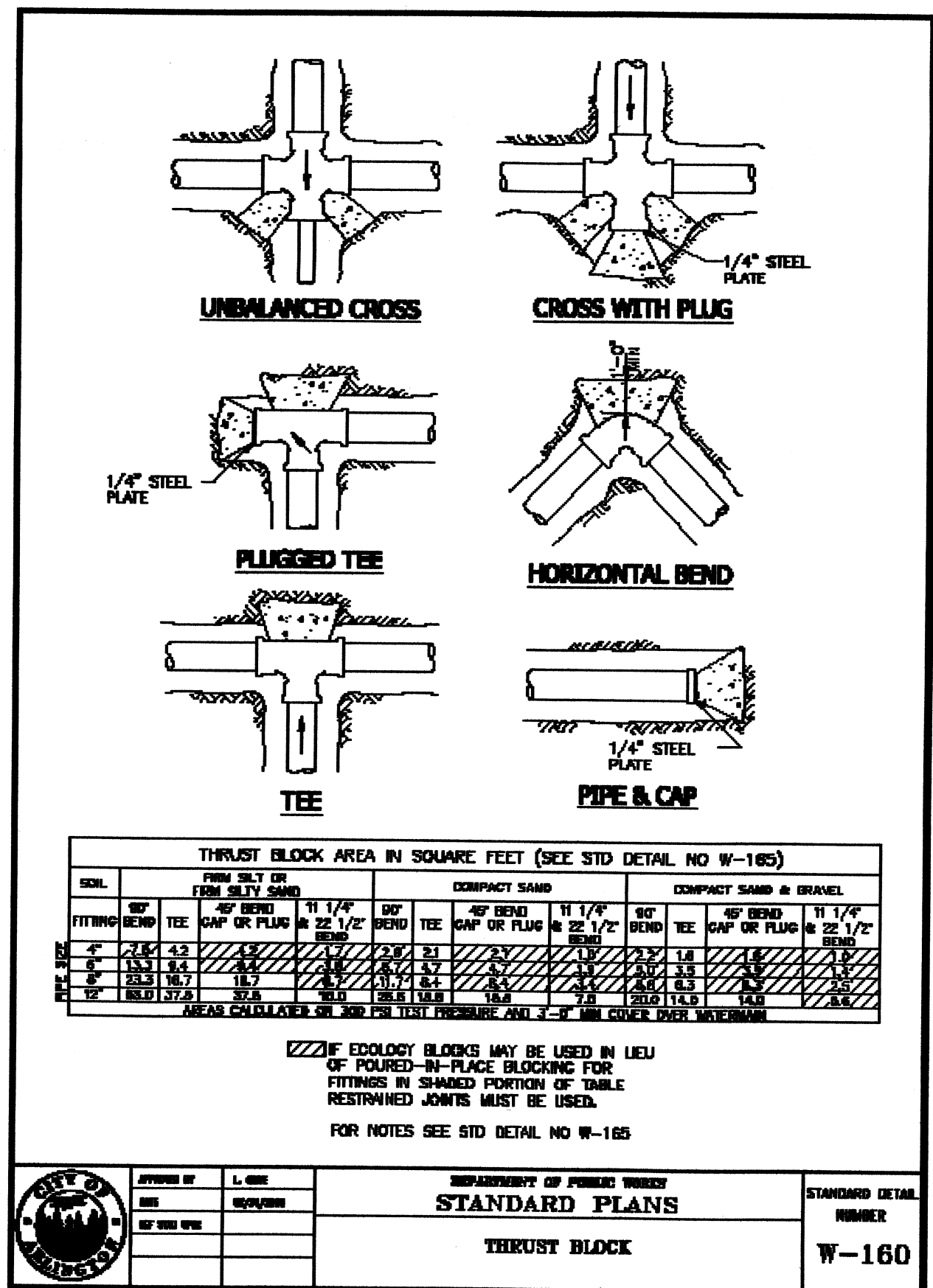
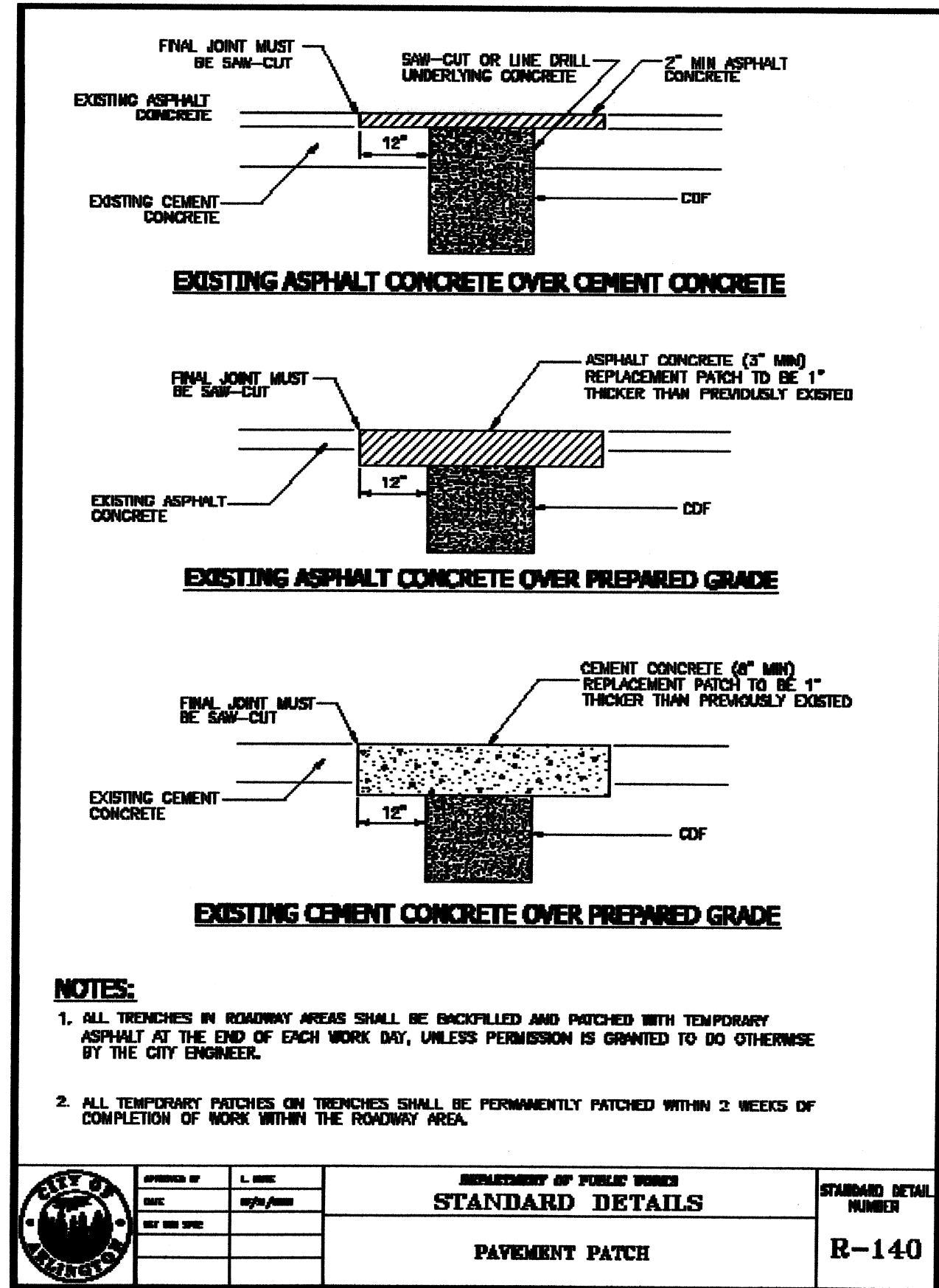
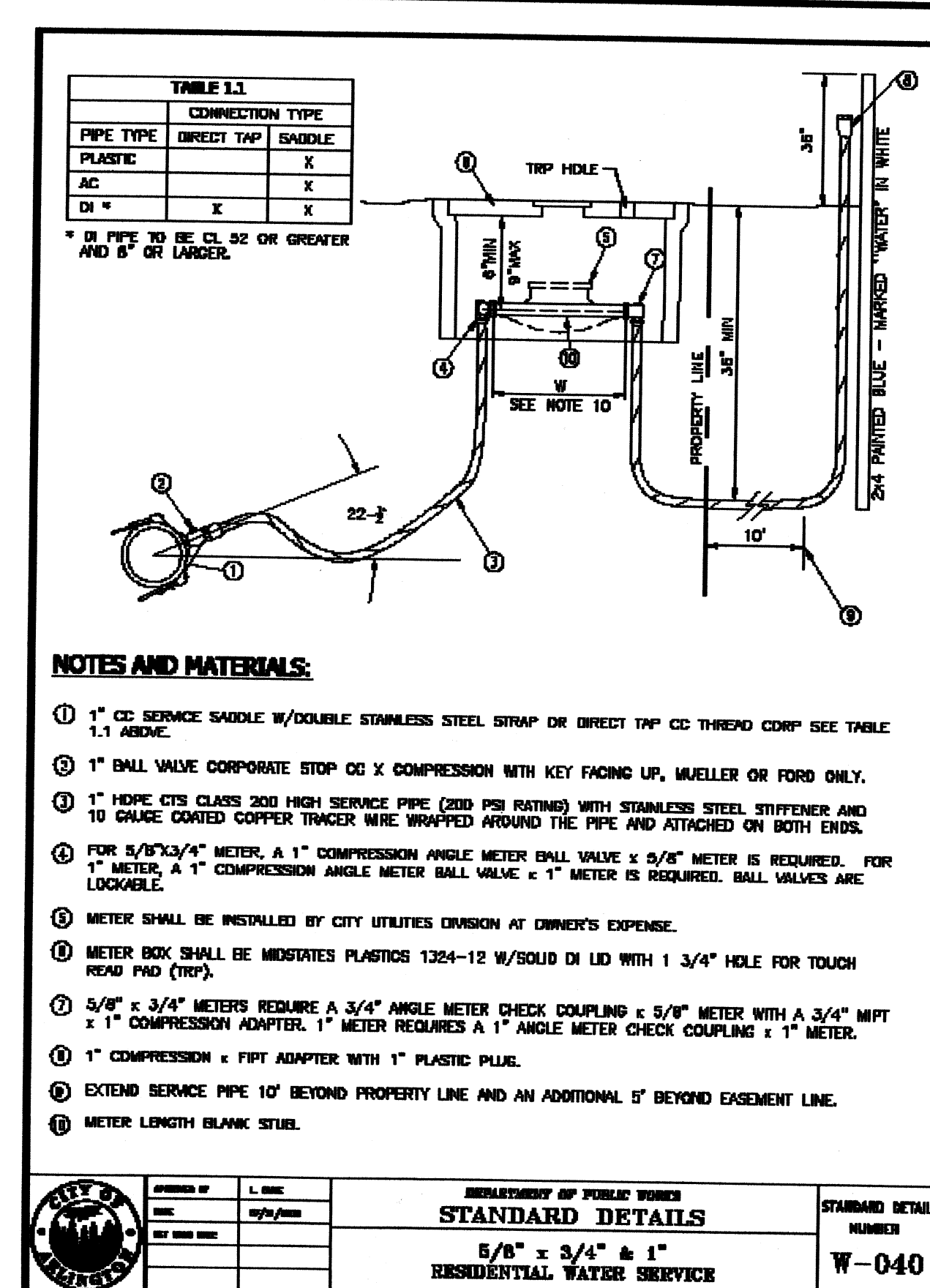
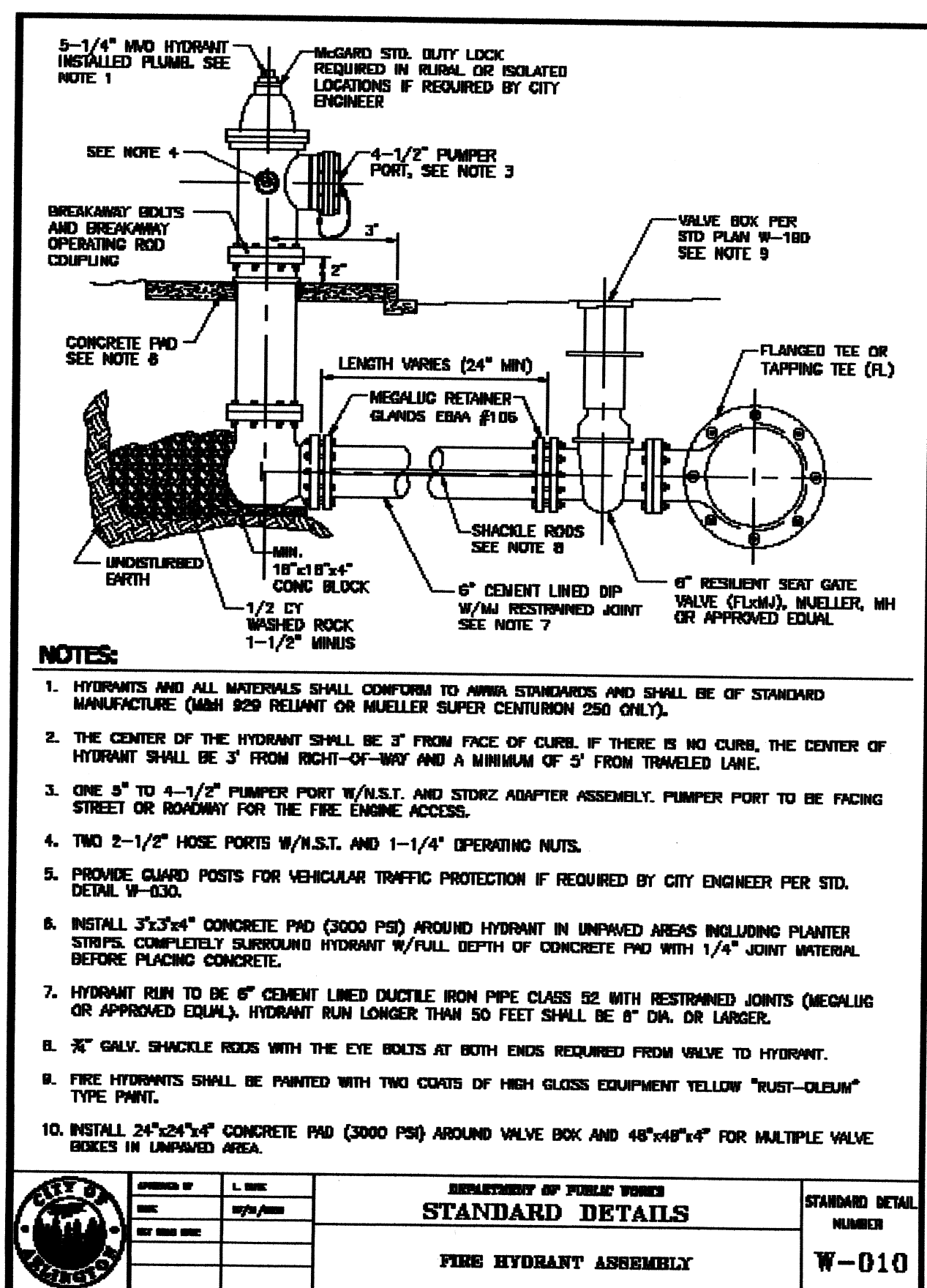
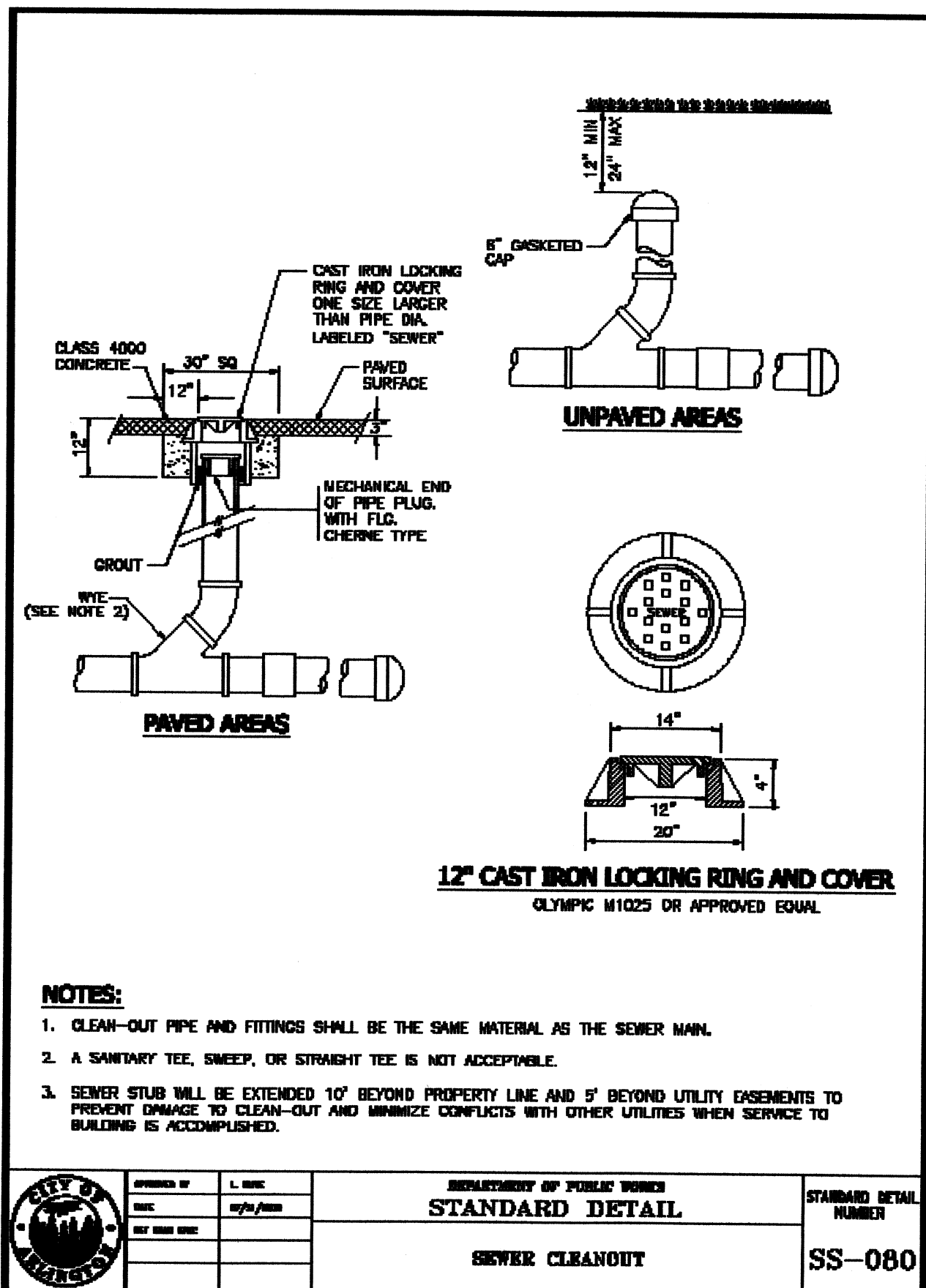
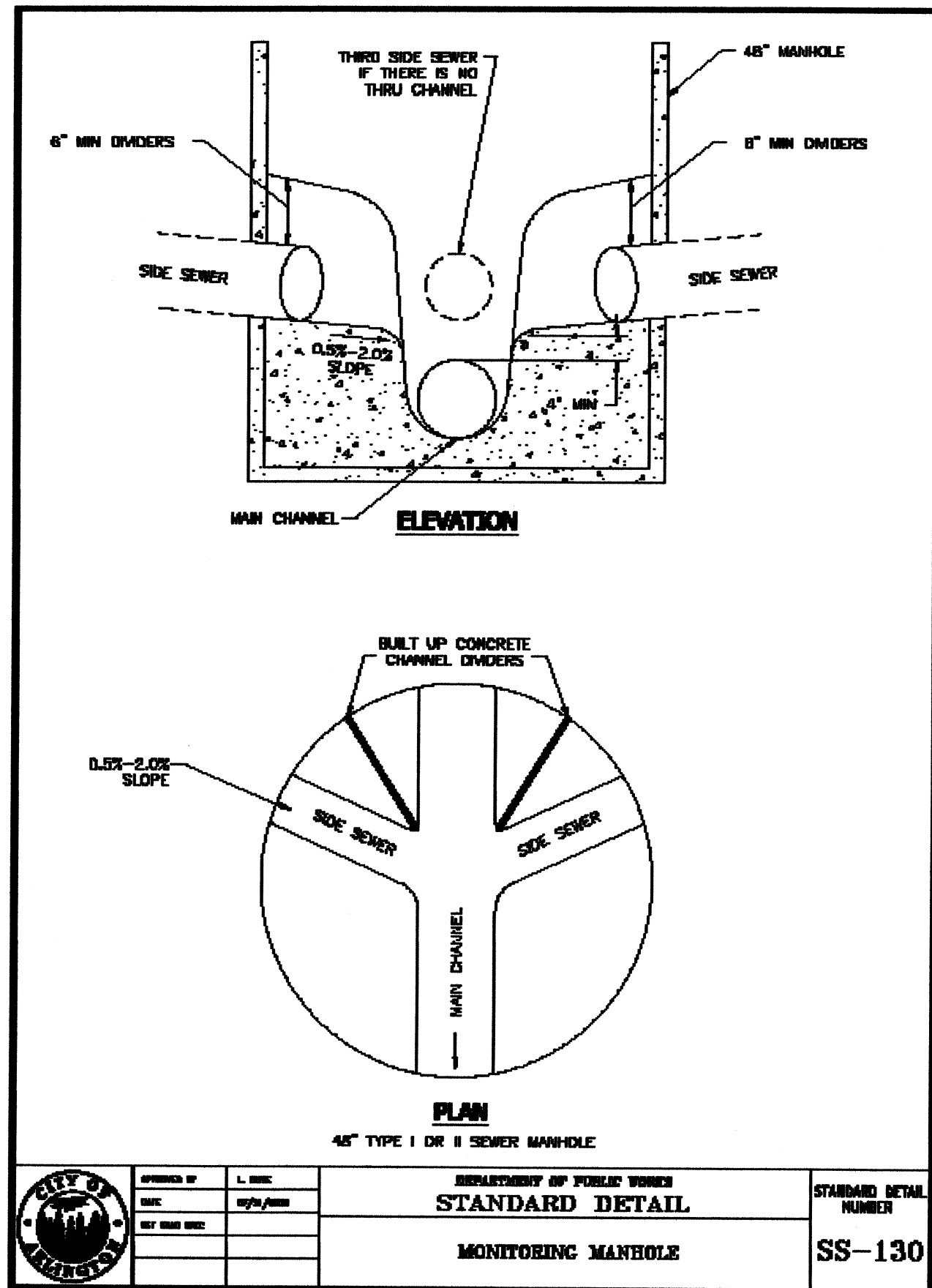
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DESIGNED BY:			KSH
DRAWN BY:			KSH/JSM



CARRERA PACIFIC HANGARS
ARLINGTON AIRPORT LOT 12 & 13
WATER & SEWER PLAN

20110017

5 OF 7



HALE MILLIGAN & ASSOCIATES
ENGINEERING
307 N. Olympic Avenue, Suite 205, Arlington, VA 22203
P: 303.474.4624 F: 303.474.4625

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DESIGNED BY: KSH
DRAWN BY: KSH/JSM

CARRERA PACIFIC HANGARS
ARLINGTON AIRPORT LOT 12 & 13
CITY OF ARLINGTON DETAILS

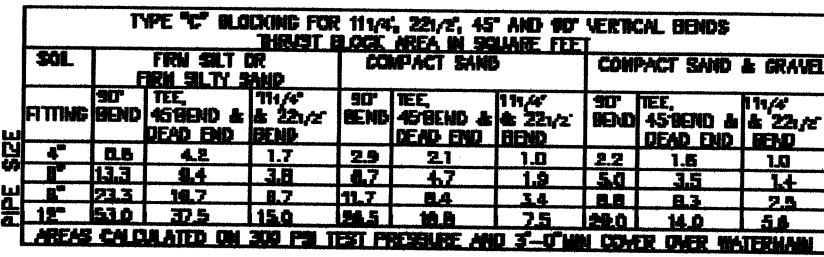
20110017
6 OF 7

AS-BUILT DRAWING APPROVAL
ACKNOWLEDGMENT

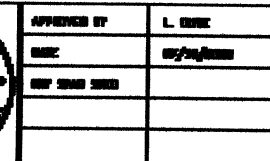
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DATE: 01-29-14

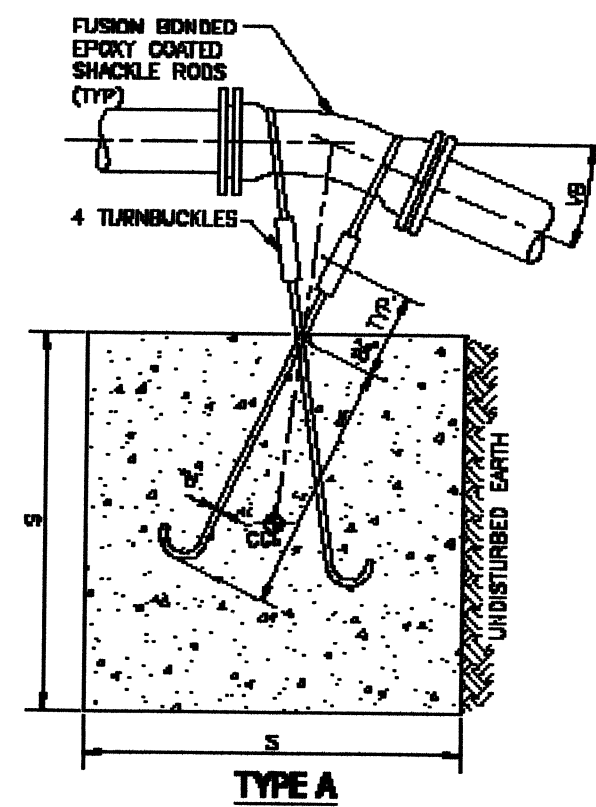


1. LOCATION AND SIZE OF BLOCKING FOR PIPE LARGER THAN 12" DIAMETER AND FOR SOIL TYPES DIFFERENT THAN SHOWN SHALL BE DETERMINED BY THE CITY ENGINEER.
2. ALL BLOCKING FOR VERTICAL COLUMNS (POURED IN PLACE) SHALL BEAR AGAINST UNDISTURBED NATIVE GROUND.
3. ALL POURED THRUST BLOCKS SHALL BE BACKFILLED AFTER MINIMUM 1 DAY. PRESSURE TESTING SHALL OCCUR AFTER CONCRETE HAS REACHED COMPRESSIVE STRENGTH (F_C).
4. ALL BLOCKING SHALL BE CONCRETE GL 3000-1 (3000 psi).
5. AFTER INSTALLATION, SHACKLE RODS & TURNBUCKLES SHALL BE CLEANED AND COATED WITH 2 COATS OF ASPHALTIC VARNISH, RUSTOL ROTKOTE #812M OR APPROVED EQUAL.
6. SHACKLE RODS SHALL BE FUSION BONDED EPOXY COATED ROUND MILD STEEL, ASTM A 36.
7. BLOCKING AGAINST FITTINGS SHALL BEAR AGAINST THE GREATEST FITTING SURFACE AREA POSSIBLE, BUT SHALL NEVER COVER EXPOSED BELL ENDS, JOINT BOLTS OR GLANDS REASONABLE ACCESS TO BOLTS AND GLANDS SHALL BE PROVIDED.

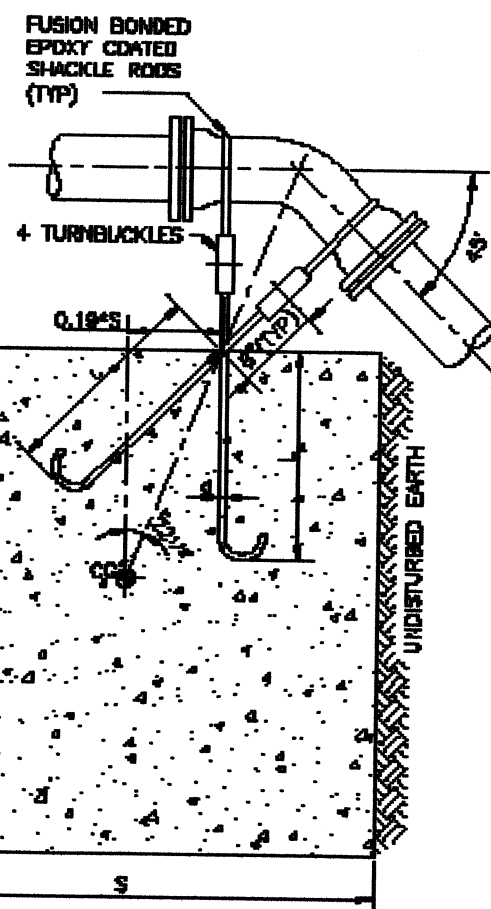


STANDARD DETAILS

STANDARD DETAIL
NUMBER
W-175



TYPE A BLOODING FOR 11" V ⁸ & 22" V ⁸ VERTICAL BENS						
		1/8"	5/8"	1"	1 1/2"	2"
PIPE SIZE NOM DIA INCHES	TEST PRESSURE PSI	VERTICAL BEND DOUBLES	NO. OF CUFFS OR CUMC BUCKING	SIZE OF CURE FEET	DIA OF SHAKABLE HOODS (3)	DEPTH OF HOODS IN INCHES
4"	300	11/14	2	2	3/4	18
		22/32	1	2	3/4	18
6"	300	11/14	2	2	3/4	24
		22/32	1	2	3/4	24
8"	300	11/14	16	2 1/2	3/4	36
		22/32	43	3 1/2	3/4	36
12"	300	11/14	125	3	1	48
		22/32	125	3	1	48



TYPE B

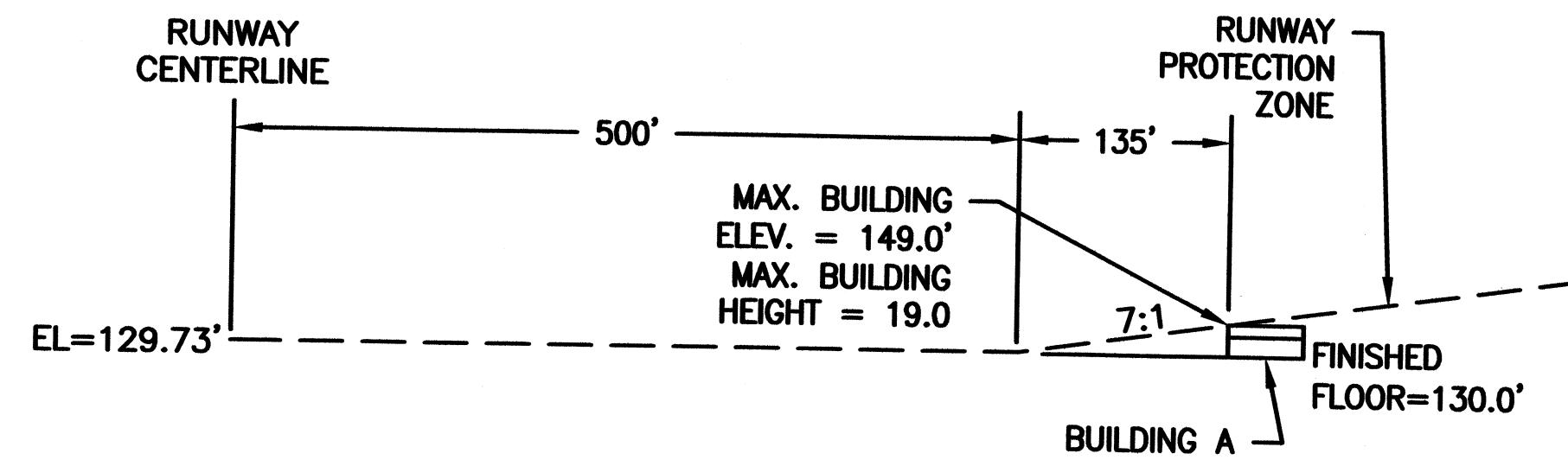
FOR NOTES SEE STD DETAIL NO. WF-175

PIPE SIZE NOM DIA (INCHES)	TEST PRESSURE (PSI)	VERTICAL BEND (INCHES)	NO OF CU FT OF CONC BLOCKING	SIZE OF CURVE (FEET)	DIA OF SHACKLE RODS (4) (INCHES)	DEPTH OF RIGGS IN CONCRETE (INCHES)
4"			27	3		
6"	300	45	64	4		
8"			125	5	3/4	8
12"			216	8	1	10

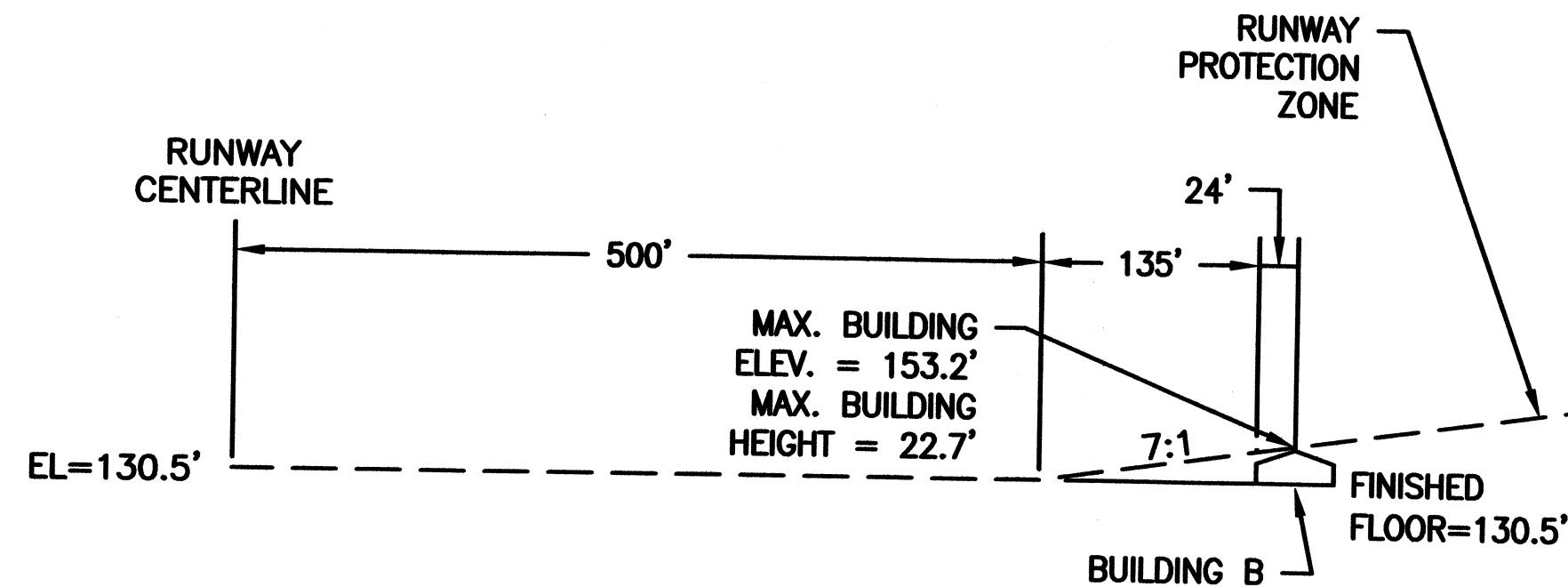


DEPARTMENT OF PUBLIC WORKS
STANDARD DETAILS
VERTICAL THRUST BLOCK
TYPE A & TYPE B

STANDARD DETAIL NUMBER	W-170
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RUNWAY PROTECTION ZONE – BUILDING "A" HEIGHT
NOT TO SCALE



RUNWAY PROTECTION ZONE – BUILDING "B" HEIGHT
NOT TO SCALE

1. POINTS AT RUNWAY AND BUILDING CORNERS PROVIDED BY HUEY SURVEYING ON 8.8.11.
2. IT IS THE CONTRACTORS RESPONSIBILITY TO VERIFY ALL ELEVATIONS PRIOR TO CONSTRUCTION.

HANGER A

WEST END ELEVATION OF PEAK OF FALSE FRONT:	149.08'
COMPUTED MAXIMUM ELEVATION PER AVIGATION EASEMENT:	149.05'

PEAK OF FALSE FRONT ELEVATION: 153.66'
COMPUTED MAXIMUM ELEVATION PER AVIGATION EASEMENT: 157.79'

HANGER B
NORTH END ROOF PEAK ELEV: 154.03'
COMPUTED MAXIMUM ELEVATION PER AVIGATION EASEMENT: 154.22'

NORTHWEST CORNER ROOF EAVE ELEV: 145.37'
COMPUTED MAXIMUM ELEVATION PER AVIGATION EASEMENT: 150.51'

MIDPOINT OF HANGER ROOF PEAK ELEV: 154.03'
COMPUTED MAXIMUM ELEVATION PER AVIGATION EASEMENT: 153.78'

WEST MIDPOINT OF HANGER EAVE ELEVATION: 145.38'
COMPUTED MAXIMUM ELEVATION PER AVIGATION EASEMENT: 150.07'

SOUTH END ROOF PEAK ELEV: 154.03'
COMPUTED MAXIMUM ELEVATION PER AVIGATION EASEMENT: 153.17'

SOUTHWEST CORNER ROOF EAVE ELEV: 145.44'
COMPUTED MAXIMUM ELEVATION PER AVIGATION EASEMENT: 149.45'

AS-BUILT DRAWING APPROVAL
ACKNOWLEDGMENT

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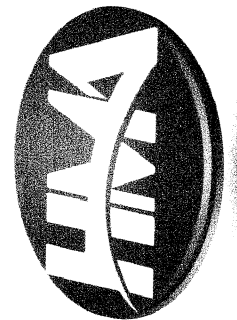
DATE: 11-29-14

**CARRERA PACIFIC HANGARS
ARLINGTON AIRPORT LOT 12 & 13
CITY OF ARLINGTON DETAILS**

20110017

7 OF 7

HALE MILLIGAN & ASSOCIATES
ENGINEERING
307 N Olympic Avenue Suite 209 Arlington, WA 98223
P-360.474.4624 F-425.968.1245



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DESIGNED BY:
KSH

DRAWN BY:
KSH/JSM

