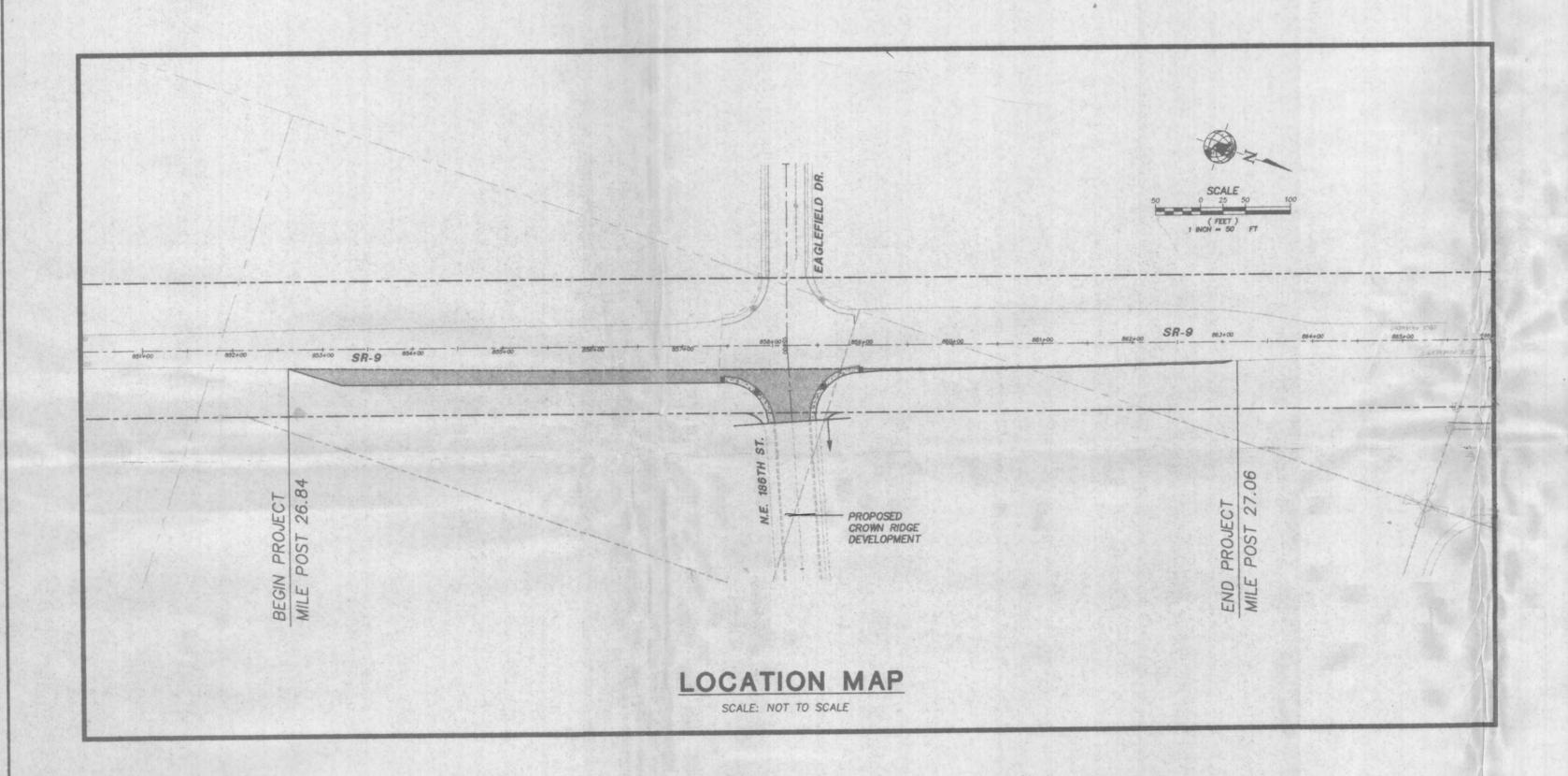
STAR DEVELOPMENT, INC. STATE ROUTE 9 IMPROVEMENTS

ARLINGTON, WASHINGTON

MARCH 1998



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VICINITY MAP

LEGEND

	-	BOUNDARY		CURB RAMP
-		LOT LINE	356	RIPRAP
	-	RIGHT-OF-WAY	.1.	SIGN
		GUTTER, FLOW, & CURB LINE	TTT	CUT SLOPES
	_	CENTER LINE		FILL SLOPES
		STORM DRAIN PIEE		
-	-	WATER MAIN		SINAW DALLS
		SWALE OR DITCH		SIDEWALK
		10' EXISTING CONTOUR	10000	
		2' EXISTING CONTOUR	9599	RIPRAP
		- DAYLIGHT LINE	GL TALL	
xx-	x-	X FILTER FENCE		

MJ 97-0005 LP RECEIVED MAR 25 1998

CITY OF ARLINGTON

50% SUBMITTAL

CROWN RIDGE ESTATES File No. MJ 97-0005-LP

MAR 2 4 1998 W & H PACIFIC

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EXPIRES 3/4/99

MARCH 20, 1998

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Planning * Engineering * Surveying
Landscape Architecture * Environmental Services

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SHEET 01.1

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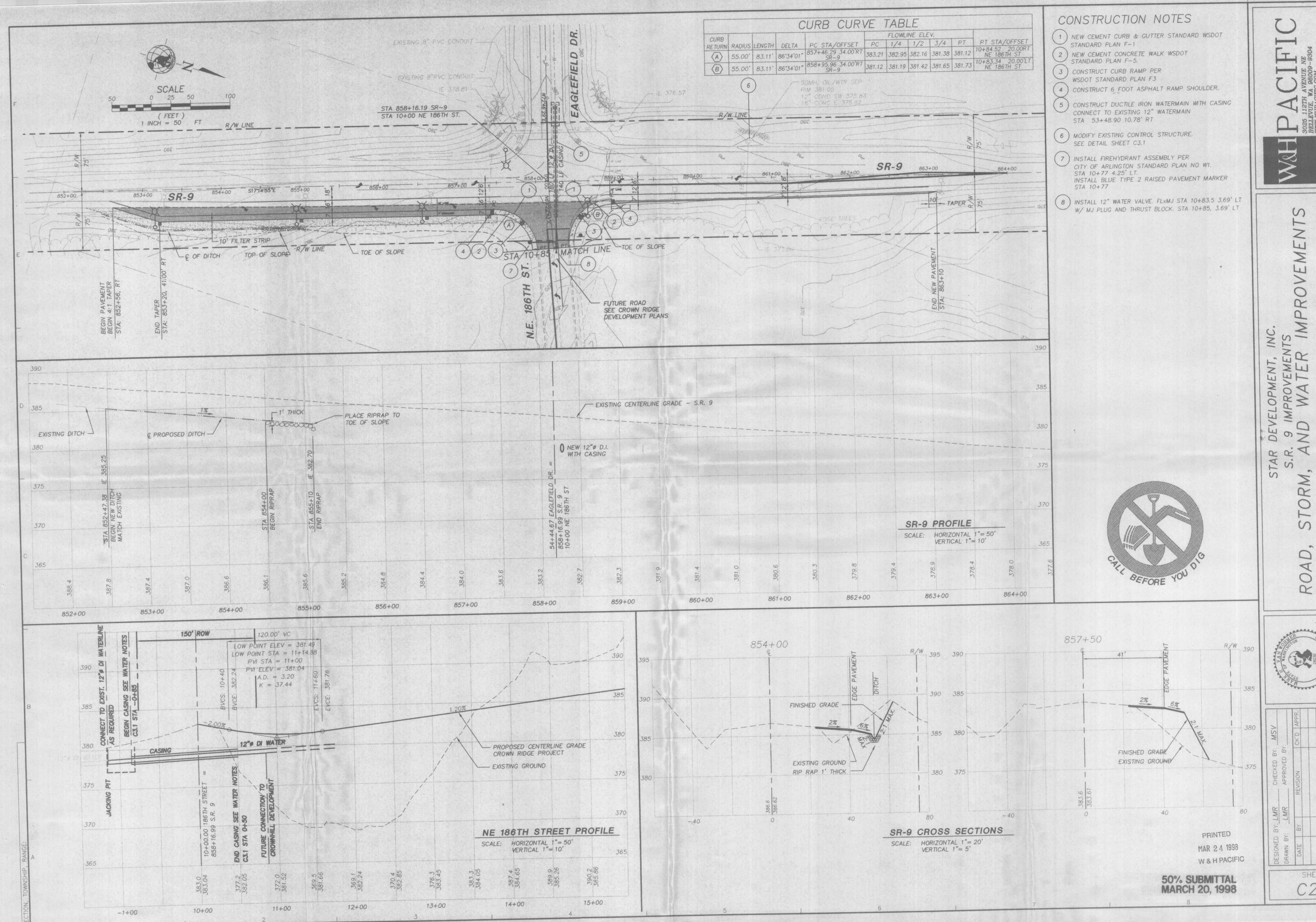
PROJECT BENCHMARK BASED ON SNOHOMISH COUNTY BENCHIARK #1294, #516, & #1325 VERTICAL DATUM - NAVD 88

ELEV. - #1294=119.36, #516=140.88, 1325=148.17 HORIZONTAL DATUM - NAD 83/91

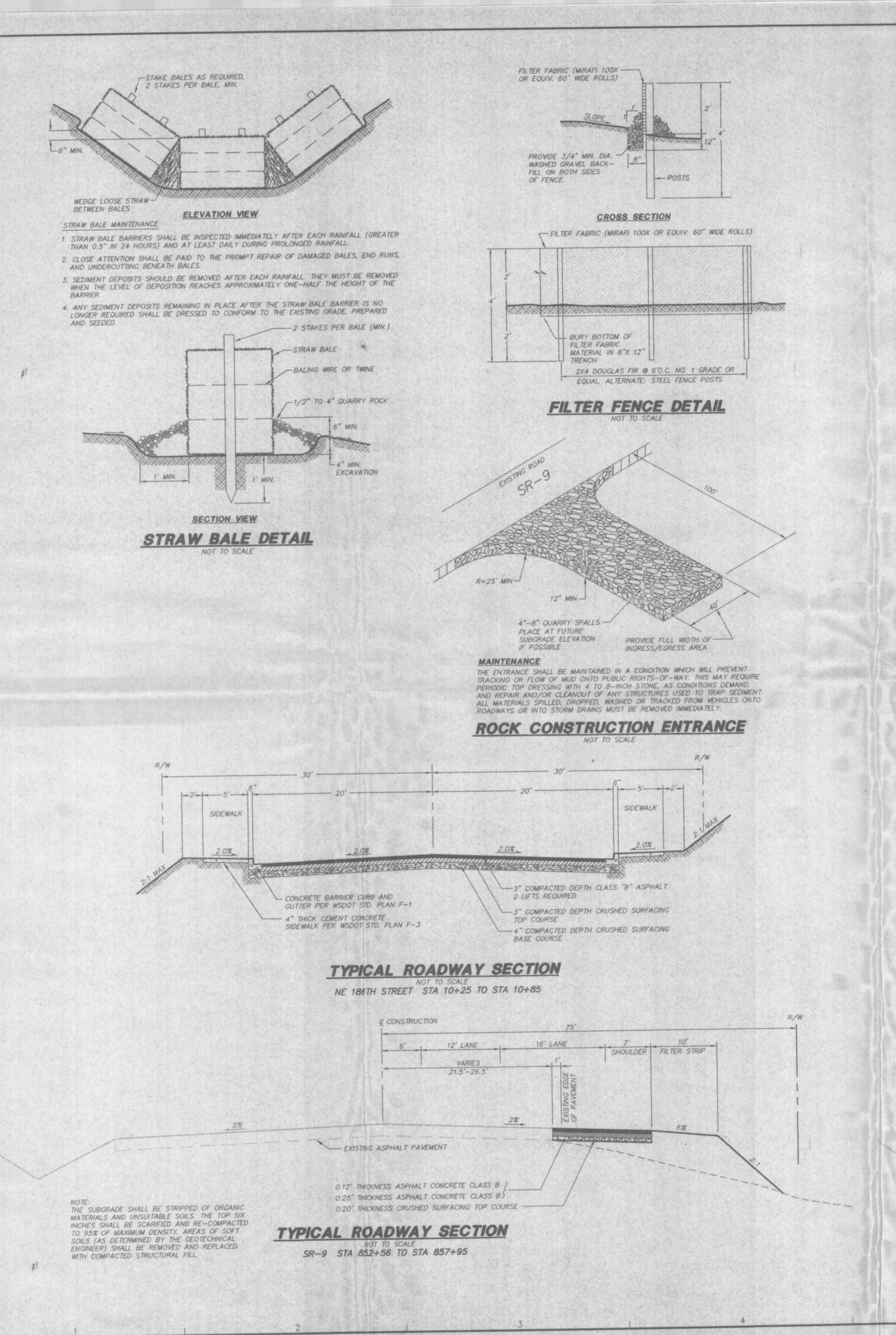
WSDOT NORTHWEST REGION

ROAD AND STORM DRAINAGE PLAN

PROJECT DEVELOPMENT ENGINEER



SHEET



GENERAL NOTES (CITY OF ARLINGTON)

1. ALL WORK AND MATERIALS SHALL BE IN ACCORDANCE WITH CITY OF ARLINGTON DESIGN AND CONSTRUCTION STANDARDS AND SPECIFICATIONS; WASHINGTON STATE CHAPTER OF AMERICAN PUBLIC WORKS ASSOCIATION (A.P.W.A) STANDARD SPECIFICATIONS FOR MUNICIPAL PUBLIC WORKS CONSTRUCTION, LATEST EDITION; WASHINGTON STATE DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS AND PLANS FOR ROAD, BRIDGE AND MUNICIPAL CONSTRUCTION, LATEST EDITION.

2. ALL WORK PERFORMED IN THE CONSTRUCTION OR IMPROVEMENT OF CITY STREETS AND ALL APPURTENANCES, WHETHER BY OR FOR A PRIVATE DEVELOPER, SHALL BE DONE TO THE SATISFACTION OF THE CITY ENGINEER AND IN ACCORDANCE WITH THE PLANS APPROVED BY THE CITY FOR THE WORK, IT IS EMPHASIZED THAT NO PERMITS SHALL BE ISSUED TO START WORK UNTIL PLANS FOR IT ARE APPROVED. ANY REVISIONS TO THE PLANS SHALL BE APPROVED BY THE CITY ENGINEER BEFORE BEING IMPLEMENTED. A SET OF "AS-BUILT" DRAWINGS (MYLARS) WILL BE REQUIRED AT THE COMPLETION OF THE PROJECT AND PRIOR TO FINAL ACCEPTANCE OF THE WORK. SEE INDIVIDUAL UTILITY SECTIONS FOR MORE SPECIFIC "AS-BUILT" REQUIREMENTS. "AS-BUILT" DRAWINGS WILL BE PREPARED BY THE ENGINEER. SUPPLEMENTAL INFORMATION SHALL BE PROVIDED BY THE CONTRACTOR TO THE ENGINEER.

3. ALL ROADWAY CONSTRUCTION INCLUDING, BUT NOT LIMITED TO, STORM DRAIN, WATER AND SANITARY SEWERS IN AND OUT OF THE RIGHT-OF-WAY WILL BE DONE UNDER THE INSPECTION OF THE CITY ENGINEER AND/OR HIS DESIGNATED INSPECTORS. THE CONTRACTOR OR DEVELOPER IS RESPONSIBLE FOR ARRANGING INSPECTIONS BY THE CITY INSPECTOR FOR ALL PORTIONS OF THE WORK DURING THE CONSTRUCTION.

- 4. PRIOR TO ANY SITE WORK PERTAINING TO DRAINAGE, THE CONTRACTOR SHALL CONTACT THE CITY ENGINEER TO SCHEDULE A PRECONSTRUCTION CONFERENCE. ENGINEERING AS-BUILTS SHALL BE REQUIRED PRIOR TO ACCEPTANCE OF PUBLIC
- 5. THE EROSION/SEDIMENTATION CONTROL FACILITIES SHALL BE CONSTRUCTED PRIOR TO ANY GRADING OR EXTENSIVE LAND CLEARING, IN ACCORDANCE WITH THE APPROVED GRADING & TEMPORARY EROSION/SEDIMENTATION CONTROL PLAN. THESE FACILITIES MUST BE SATISFACTORILY MAINTAINED UNTIL CONSTRUCTION AND LANDSCAPING IS COMPLETED AND THE POTENTIAL FOR ONSITE
- 6. ALL STRUCTURAL FILLS SHALL BE COMPACTED TO A MINIMUM OF 95% OF MAXIMUM DENSITY BY MODIFIED PROCTOR TEST.
- 7. BACKFILL TRENCH FOR NEW UTILITIES SHALL BE COMPACTED TO 95% RELATIVE DENSITY UNDER ROADWAYS AND 85% RELATIVE DENSITY OFF ROADWAYS.
- 8. WATER MAINS SHALL BE FLUSHED AND CLEANED, PRIOR TO CITY OF ARLINGTON ACCEPTANCE. MAINS SHALL ALSO BE PRESSURE-TESTED PRIOR TO ACCEPTANCE.
- 9. PROVIDE AND MAINTAIN THE SEDIMENTATION COLLECTION FACILITIES TO INSURE SEDIMENT LADEN WATERS TO NOT ENTER THE NATURAL DRAINAGE SYSTEM.
- 10. ALL DISTURBED AREAS SUCH AS DETENTION FACILITIES, ROADWAY BACK-SLOPES, ETC. SHALL BE SEEDED WITH A PERENNIAL GROUND COVER GRASS TO MINIMIZE EROSION. GRASS SEEDING WILL BE DONE USING AN APPROVED HYDROSEEDER OR AS OTHERWISE APPROVED BY THE CITY OF ARLINGTON ENGINEERING DEPARTMENT.
- 11. STOCKPILES ARE TO BE LOCATED IN SAFE AREAS AND ADEQUATELY PROTECTED BY TEMPORARY SEEDING AND MULCHING. HYDRO-SEED PREFERRED.
- 12. IMMEDIATELY FOLLOWING FINISH GRADING, PERMANENT VEGETATION WILL BE APPLIED. (MINIMUM 80# PER ACRE.) THIS IS TO INCLUDE THE FOLLOWING: 20% ANNUAL, PERENNIAL OR HYBRID RYE GRASS; 40% CREEPING RED FESQUE; AND 40% WHITE CLOVER

HYDRO-SEED PREFERRED.

- 13. FERTILIZER SHALL BE APPLIED AT 400# PER ACRE OF 10-20-20 (TEN (10) POUNDS PER 1,100 SOUARE FEET) OR EQUIVALENT.
- 14. PREPARATION OF SURFACE: ALL AREAS TO BE SEEDED SHALL BE CULTIVATED TO THE SATISFACTION OF THE CITY ENGINEER. THIS MAY BE ACCOMPLISHED BY DICING, RAKING, HARROWING OR OTHER ACCEPTABLE MEANS.

CONSTRUCTION SEQUENCE (ALL WORK SHALL OCCUR IN THE FOLLOWING SEQUENCE)

- THE CONTRACTOR SHALL ARRANGE AND ATTEND A PRECONSTRUCTION MEETING WITH THE CITY OF ARLINGTON ENGINEER, INSPECTOR AND REPRESENTATIVES FROM W.S.D.O.T.
- BY LAW THE UNDERGROUND UTILITY LOCATION SERVICE SHALL BE CONTACTED AT LEAST (10) DAYS PRIOR TO ANY CONSTUCTION (PHONE 1-800-424-5555). THE ENGINEER SHALL BE NOTIFIED IMMEDIATLY IF CONSTRUCTION CONFLICTS ARE

3. CLEARLY FLAG ALL CLEARING LIMITS.

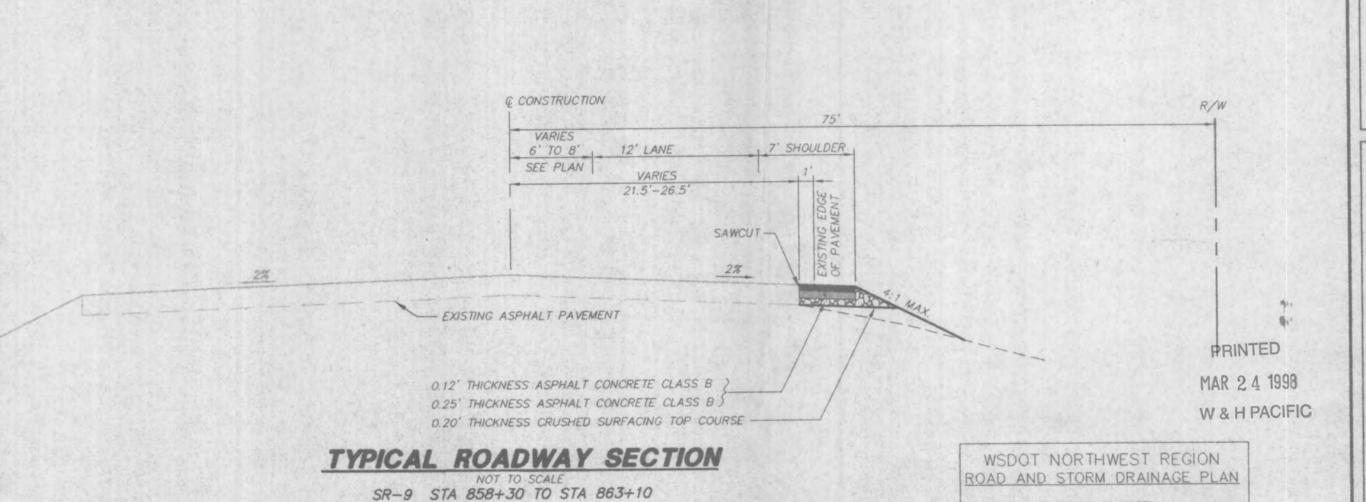
- 4. INSTALL ALL EROSION CONTROL FACILITIES, AS SHOWN ON THE PLANS. ADDITIONAL FACILITIES MAY BE REQUIRED DUE TO WEATHER CONDITIONS AND ADDITIONAL CLEARING REQUIREMENTS. THE CONTRACTOR SHALL NOT BEGIN CLEARING OR GRADING OPERATIONS UNTIL EROSION CONTROL FACILITIES HAVE BEEN INSPECTED AND APPROVED BY THE
- 5. TRAFFIC CIRCULATION SHALL BE MAINTAINED THROUGHOUT CONSTRUCTION ON SR-9, SEE TRAFFIC CONTROL NOTES.
- 6. CLEAR, GRUB AND GRADE THE SITE TO SUBGRADE, AS SPECIFIED IN THESE PLANS. COMPACTION AND SOIL STABILITY SHALL BE APPROVED BY THE GEOTECHNICAL ENGINEER. ALL DISTURBED SURFACES SHALL BE HYDROSEEDED.
- 7. INSTALL ALL UTILITIES, INCLUDING STORM DRAINAGE, ILLUMINATION, AND WATER. ADDITIONAL UTILITIES SUCH AS POWER, GAS AND CABLE T.V. INSTALLATION WILL NEED TO BE COORDINATED EDBY CONTRACTOR.
- 8. WIDEN ROAD AND PAVE.
- 9. REMOVE FILTER FENCE AND/OR STRAW BALES. CLEAN OUT STORM SYSTEM IF REQUIRED

GENERAL NOTES (W.S.D.O.T.)

- 1. BEFORE ANY CONSTRUCTION/DEVELOPMENT ACTIVITY OCCURS, A PRECONSTRUCTION MEETING MUST BE HELD WITH WSDOT.
- 2. A COPY OF THE APPROVED PLANS MUST BE ON THE JOB SITE WHENEVER CONSTRUCTION IS IN PROGRESS.
- 3. ALL DISTURBED AREAS SHALL BE SEEDED AND MULCHED OR SIMILARLY STABILIZED TO THE SATISFACTION OF WSDOT FOR THE PREVENTION OF ON-SITE EROSION AFTER THE COMPLETION OF CONSTRUCTION.
- 4. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH WSDOT/APWA 1998 STANDARDS AND ACCORDING TO THESE APPROVED PLANS UNLESS MODIFIED BY
- 5. COMPACTION OF SUBGRADES FOR ALL ROADWAYS SHALL COMPLY WITH COMPACTION REQUIREMENTS OF WSDOT MAINTAINED ROADWAYS. THE SUBGRADES UNDER ALL AREAS TO BE PAVED SHALL BE FIRM AND UNYIELDING, AND COMPACTED TO 95% OF THE MAXIMUM DENSITY (PER STANDARD PROCTOR TEST). PROOF ROLLING MAY BE REQUIRED PRIOR TO PAVING TO INSURE STABILITY. COMPACTION OF ALL FILL AREAS SHALL BE PER WSDOT/APWA 1998 SPECIFICATIONS. FILL SHALL BE PROVIDED IN 6-INCH MAXIMUM LIFTS AND SHALL BE COMPACTED TO 95% OF ITS MAXIMUM RELATIVE
- 6. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING ADEQUATE SAFEGUARDS, SAFETY DEVICES, PROTECTIVE EQUIPMENT, FLAGGERS, AND ANY OTHER NEEDED ACTIONS TO PROTECT LIFE, HEALTH, AND SAFETY OF THE PUBLIC AND TO PROTECT PROPERTY IN CONNECTION WITH THE PERFORMANCE OF WORK COVERED BY THE CONTRACT. ANY WORK WITHIN THE TRAVELED RIGHT-OF-WAY THAT MAY INTERRUPT NORMAL TRAFFIC FLOW SHALL REQUIRE AT LEAST ONE FLAGGER FOR EACH LANE OF TRAFFIC AFFECTED. ALL SECTIONS OF THE W.S.D.O.T. STANDARD SPECIFICATIONS 1-07.23 TRAFFIC CONTROL SHALL APPLY.
- APPROXIMATE LOCATIONS OF EXISTING UTILITIES HAVE BEEN OBTAINED FROM AVAILABLE RECORDS AND ARE SHOWN FOR CONVENIENCE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFICATION OF LOCATIONS SHOWN AND FOR DISCOVERY OF POSSIBLE ADDITIONAL UTILITIES NOT SHOWN SO AS TO AVOID DAMAGE OR DISTURBANCE. THE CONTRACTOR SHALL CONTACT THE UTILITIES UNDERGROUND LOCATION SERVICE (1-800-424-5555) PRIOR TO CONSTRUCTION. THE OWNER OR HIS REPRESENTATIVE SHALL BE IMMEDIATELY CONTACTED IF A UTILITY CONFLICT EXISTS. THE CONTRACTOR SHALL BE RESPONSIBLE TO ENSURE THAT UTILITY LOCATES ARE MAINTAINED THROUGHOUT THE LIFE OF THE PROJECT.
- 8. THE CONTRACTOR SHALL VERIFY THE LOCATIONS, WIDTHS, THICKNESSES, AND ELEVATIONS OF ALL EXISTING PAVEMENTS AND STRUCTURES THAT ARE TO INTERFACE WITH NEW WORK. PROVIDE ALL TRIMMING, CUTTING, SAW CUTTING, GRADING, LEVELING, SLOPING, COATING AND OTHER WORK, INCLUDING MATERIALS AS NECESSARY, TO CAUSE THE INTERFACE WITH EXISTING WORK TO BE PROPER, ACCEPTABLE TO THE ENGINEER, AND WSDOT COMPLETE AND IN PLACE AND READY TO USE.
- CONSTRUCTION DRAINAGE CONTROL SHALL BE MAINTAINED BY THE DEVELOPER AND WILL BE SUBJECT TO PERIODIC INSPECTIONS. DURING THE PERIOD FROM APRIL 1 TO OCTOBER 31, ALL DENUDED SOILS MUST BE COVERED WITHIN 15 DAYS; BETWEEN NOVEMBER 1 AND MARCH 31, ALL DENUDED SOILS MUST BE COVERED WITHIN 12 HOURS.

TRAFFIC CONTROL NOTES (DURING CONSTRUCTION)

- 1. THE CONTRACTOR SHALL SUBMIT A TRAFFIC CONTROL PLAN TO WASHINGTON STATE DEPARTMENT OF TRANSPORTATION (WSDOT) FOR APPROVAL. THESE PLANS SHALL BE SUBMITTED FOR APPROVAL AT LEAST 15 WORKING DAYS BEFORE BEGINNING ANY CONSTRUCTION. PLANS SHALL BE DEVELOPED BY THE CONTRACTOR AND MUST CONFORM TO THE ESTABLISHED STANDARDS OF THE MUTCO AND WSDOT.
- DELAYS TO TRAFFIC SHALL BE HELD TO A MINIMUM. THERE SHALL BE NO RESTRICTIONS OR INTERRUPTIONS TO TRAFFIC ON FRIDAY THROUGH SUNDAY. IN ADDITION, THERE SHALL BE NO RESTRICTIONS OR INTERRUPTIONS TO TRAFFIC FROM 12:00 NOON ON THE DAY PRIOR TO A HOLIDAY OR HOLIDAY WEEKEND, THROUGH THE LAST DAY OF THE HOLIDAY OR HOLIDAY WEEKEND.
- 3. SUBJECT TO THE ABOVE TRAFFIC RESTRICTIONS, WORK OPERATIONS WILL BE LIMITED TO DAYLIGHT HOURS, MONDAY THROUGH FRIDAY.
- 4. DURING NONWORKING HOURS, THE CONTRACTOR SHALL KEEP ALL LANES OPEN TO TRAFFIC THROUGHOUT THE LIMITS OF THE PROJECT. ALL LANES SHALL BE COMPLETELY CLEAR OF ALL MATERIALS, TOOLS, DEBRIS, PERSONNEL AND EQUIPMENT. SHOULDERS SHALL NOT BE USED AS STAGING OR STORAGE AREAS BY THE CONTRACTOR.
- 5. SPECIAL EVENTS THAT GENERATE INCREASED TRAFFIC VOLUMES THROUGH THE WORK AREA MAY OCCUR DURING THE LIFE OF THIS PROJECT. LANE RESTRICTIONS MAY BE DENIED IF SEVERE TRAFFIC CONGESTION IS EXPECTED.
- 6. THERE SHALL BE NO DELAY TO MEDICAL, FIRE, POLICE, OR OTHER EMERGENCY VEHICLES WITH FLASHING LIGHTS OR SIRENS. THE CONTRACTOR SHALL ALERT ALL FLAGGERS AND PERSONNEL OF THIS REQUIREMENT.
- 7. THE CONTRACTORS WORK VEHICLE MOVEMENTS SHALL BE CONDUCTED AS NORMAL AND LEGAL TRAFFIC MOVEMENTS. THE CONTRACTORS INGRESS AND EGRESS OF THE WORK AREA SHALL BE ACCOMPLISHED WITH AS LITTLE DISRUPTION TO TRAFFIC AS
- 8. TRAFFIC CONES USED FOR TRAFFIC CONTROL ON THIS PROJECT SHALL BE A MINIMUM
- OF 28 INCHES IN HEIGHT, UNLESS OTHERWISE APPROVED BY WSDOT. 9. EXISTING STRIPING SHALL REMAIN AS IS AND THREE TYPE III BARRICADES SHALL BE PLACED AT THE INTERSECTION TO KEEP EAGLEFIELD DRIVE CLOSED UNTIL W.S.D.O.T. COMPLETES THE OVERLAY AND NEW STRIPING.
- 10. UPON COMPLETION OF THE CONTRACT, ALL TRAFFIC CONTROL DEVICES AND EQUIPMENT, EXCEPT THAT FURNISHED BY THE STATE, SHALL REMAIN THE PROPERTY OF THE CONTRACTOR AND SHALL BE REMOVED BY THE CONTRACTOR FROM THE PROJECT AT NO COST TO THE STATE. ANY DEBRIS THAT HAS ACCUMULATED NEXT TO ANY TRAFFIC CONTROL DEVICE SHALL BE REMOVED BY THE CONTRACTOR, AT THE CONTRACTORS EXPENSE, AND TO THE SATISFACTION OF WSDOT.



PROJECT DEVELOPMENT ENGINEER APPROVED_____ DATE ___

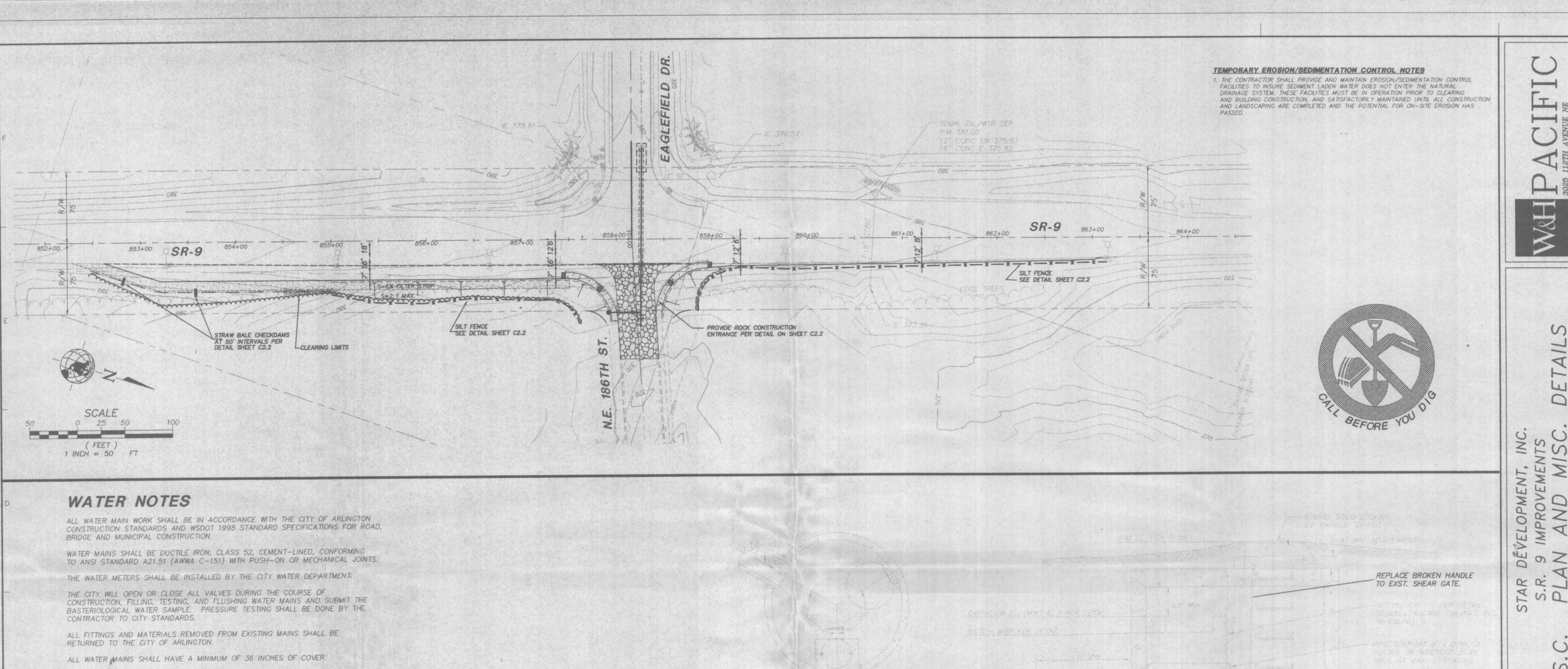
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THRUST BLOCKING IS REQUIRED AT ANGLE POINTS AROUND BENDS AND SHALL CONFORM TO MANUFACTURER'S RECOMMENDATIONS AND REQUIREMENTS OF THE CITY UTILITIES DEPARTMENT.

PIPE LENGTHS SHOWN ARE APPROXIMATE. CONTRACTOR IS RESPONSIBLE FOR SUPPLYING PROPER QUANTITIES OF MATERIALS.

CASING PIPE INSTALLATION

WHERE SHOWN IN THE PLANS, THE CONTRACTOR SHALL INSTALL CASING PIPE FOR THE PIPELINES I TRENCHES OR BY TUNNELING, JACKING, OR AUGURING WITHOUT DAMAGE TO EXISTING STREETS, ROADS, HIGHWAYS, OR OTHER UTILITIES.

MATERIALS

CASING PIPE SHALL BE STEEL OR REINFORCED CONCRETE AS FOLLOWS:

STEEL PIPE

STEEL PIPE SHALL BE MINIMUM DIAMETER OF 20 INCHES WITH A MINIMUM WALL THICKNESS OF 0.406 INCHES AND DESIGNED FOR A MINIMUM YIELD STRENGTH OF 35,000 PSI. ALL JOINTS SHALL BE WELDED BY OPERATORS WHO HAVE BEEN QUALIFIED BY TESTS AS PRESCRIBED BY THE AWS IN STANDARD QUALIFICATIONS PROCEDURE TO PERFORM THE TYPE OF WORK REQUIRED. THE QUALITY OF WELDING SHALL CONFORM TO AWS D1.1-80 STRUCTURAL WELDING CODE, SECTION 3, WORKKMANSHIP.

CONSTRUCTION REQUIREMENTS

BEFORE PLACING THE CASING AND PIPELINE, THE CONTRACTOR SHALL SUBMIT TO THE ENGINEER, FOR APPROVAL, AN OUTLINE OF THE CONSTRUCTION PROCEDURE TOGETHER WITH A LISTING OF THE EQUIPMENT FOR THE WORK. THE APPROVAL OF PROCEDURE AND EQUIPMENT BY THE ENGINEER SHALL NOT RELIEVE THE CONTRACTOR OF RESPONSIBILITY, OR WAIVE, OR MODIFY ANY OF THE PROVISIONS OF THE CONTRACT.

SPECIAL CARE SHALL BE TAKEN DURING THE INSTALLATION OF THE PIPE TO ENSURE THAT NO SETTLEMENT OF ADJACENT AREAS OCCURS. ANY SUCH SETTLEMENT CAUSED BY THE PLACEMENT OF THE PIPE SHALL BE THE CONTRACTOR'S RESPONSIBILITY. THE CONTRACTOR SHALL REPAIR ALL AREAS SO AFFECTED, AS DIRECTED BY THE ENGINEER. IN THE EVENT THAT THE PIPE IS DAMAGED DURING THE INSTALLATION AND THE DEFECTS CANNOT BE CORRECTED TO THE SATISFACTION OF THE ENGINEER, THE CONTRACTOR SHALL BE REQUIRED TO REMOVE AND REPLACE THE PIPE.

THE WATER MAIN SHALL BE INSTALLED IN THE CASING USING CREOSOTE-TREATED DOUGLAS FIR SKIDS OR ALTERNATE ACCEPTABLE TO THE ENGINEER. THE FIRST PIPE JOINT OUTSIDE ALL CASTINGS SHALL BE WITHIN 12 INCHES OF THEEND OF THE CASING. AFTER THE LINES HAVE BEEN PRESSURE TESTED, THE VOID BETWEEN THE WATER MAIN AND THE CASING PIPE SHALL BE FILLED COMPLETELY WITH SAND. CEMENT GROUT PLUGS SHALL BE PLACED AT EACH END OF THE CASINGPIPE, UNLESS OTHERWISE INDICATED ON THE DRAWINGS. ALL WATER MAIN IN CASING SHALL HAVE RESTRAINED JOINTS

CB 1 PLAN VIEW

-CUT NOTCH WIER OPENING DOWN TO ELEV. = 375.38 APPLY GALVANIZED PAINT TO CUT SURFACE.

ALL STEEL PARTS AND SURFACES

MUST BE CALVANIZED AND ASPIRALT

-PLUG EXST. ORIFICE DRILL NEW 1.1 INCH DIA. ORIFICE

CB 1 RESTRICTOR

PROFILE VIEW

ADJUSTED EXISTING FLOW RESTRICTOR

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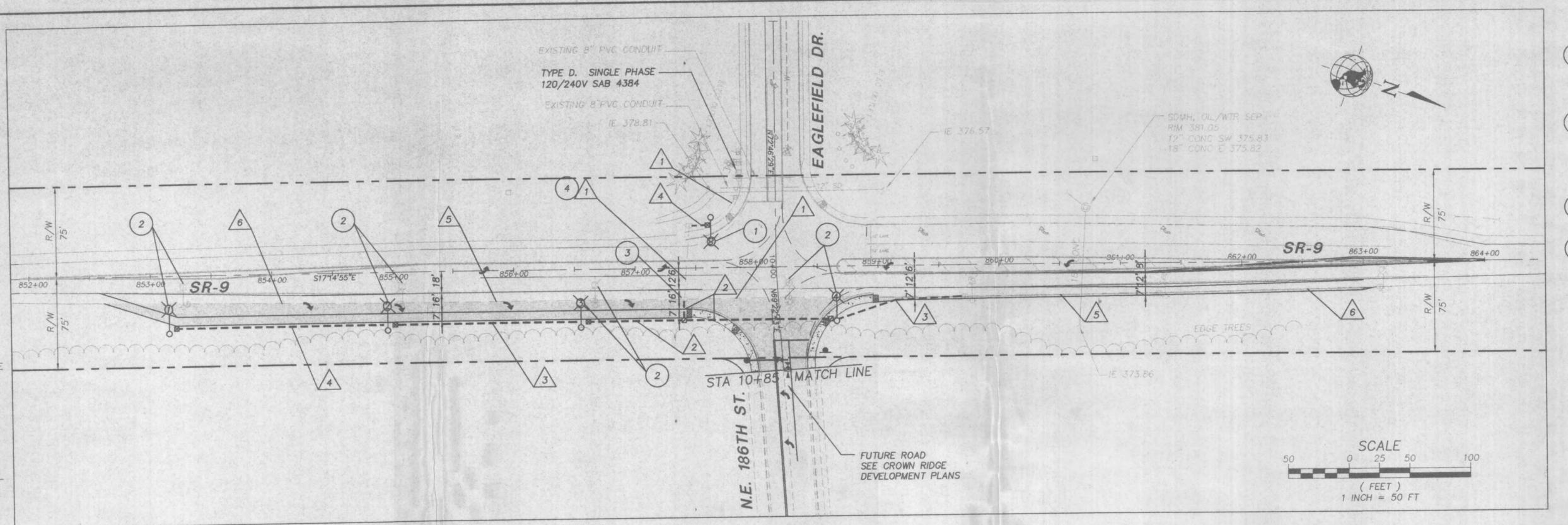
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SHEET C3.



		WIRING SCH	EDULE
WIRE NOTE	CONDUIT	NEW/EXISTING	CONDUCTORS
1	2"	EXISTING EXISTING	5 - # 8 SPARE W/ PULLSTRING
2	2"	NEW NEW	5 - # 8 SPARE W/ PULLSTRING
3	2"	NEW	5 - # 8
4	2"	NEW	3-#8
25	2"	EXISTING	5 - #8
6	2"	EXISTING	3 - #8

PRE-APPROVED PLAN

DRAWING NO. DB00013-RM SHEETS 1, 2, &3

DRAWING NO. 3721

DRAWING NO. NWS2309 OR 2391, BOTH REV. 8-25-95

VALMONT IND. INC.

FABRICATOR

AMERON POLE

PROD. DIV.

NORTHWEST SIGNAL SUPPLY INC.

			POLE AND	LUMINAIR	RE SCHEDULE			
LUMINAIRE NO.	CIRCUIT NO.	STATUS	LOCATION	OFFSET	TYPE-DISTR-LAMP	ARM LENGTH	H-1 HEIGHT	BASE TYPE
1	S-1	RELOCATE	853+17	45' RT	III-MC-310W H.P.S.	16'	40'	SLIP
2	5-2	RELOCATE	854+97	45' RT	III-MC-310W H.P.S.	16'	40'	SLIP
3	S-1	RELOCATE	856+57	45' RT	III-MC-310W H.P.S.	16'	40'	SLIP
4	S-2	RELOCATE	858+68	45' RT	III-MC-400W H.P.S.	16'	40'	SLIP
5	S-1	NO CHANGE	859+77	30' RT	III-MC-310W H.P.S.	16'	40'	SLIP
6	S-2	NO CHANGE	863+17	29' RT	III-MC-310W H.P.S.	16'	40'	SLIP
7	S-1	NO CHANGE	861+37	25' RT	III-MC-310W H.P.S.	16'	40'	SLIP
8	S-2	NEW	857+67	39' LT	III-MC-400W H.P.S.	16'	40'	SLIP

CONSTRUCTION NOTES

- INSTALL NEW TYPE I STREETLIGHT PER WSDOT STANDARD
 DETAIL J-1 WITH "COBRA" STYLE LUMINAIRE TO MATCH
 EXISTING STREETLIGHTS. SEE POLE AND LUMINAIRE SCHEDULE
 AND PRE-APPROVED FABRICATOR TABLE.
- RELOCATE EXISTING STREETLIGHT. REMOVE EXISITNG
 J—BOX AND FOUNDATION. ABANDON EXISTING CONDUIT
 BURIED GREATER THAN 18 INCHES DEEP. INSTALL
 STREETLIGHT IN NEW LOCATION AND CONNECT TO EXISTING
 CIRCUITS. SEE POLE AND LUMINAIRE SCHEDULE AND WIRING SCHEDULE.
- REMOVE EXISTNG J-BOX. EXTEND CONDUIT TO NEW TYPE II
- 4 PULL NEW CONDUCTORS AND SPLICE INTO EXISITNG STREETLIGHT CIRCUITS. SEE WIRING SCHEDULE.

GENERAL NOTES

- 1. ALL MATERIAL AND CONSTRUCTION WILL BE PER WSDOT STANDARDS.
- 2. ALL POLE BASES ARE PER WSDOT STANDARD DETAIL J-1B.
- 3. ALL J-BOXES ARE PER WSDOT STANDARD DETAIL J-11A.
- 4. ALL CONDUITS ARE GALVANIZED STEEL.

LEGEND

EXISTING NEW

00 STREETLIGHT TYPE I J-BOX TYPE II J-BOX CONDUIT -

CONSTRUCTION NOTE

WIRE NOTE

SERVICE CABINET NEW PAVEMENT

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